



**Highway Factors Group Attachment - Plans for Constructing the Interstate Route 508 and
Reconstructing a Portion of the Oakhill Street, SH 9298, F.A. project number I-88-1**

Schoharie, NY

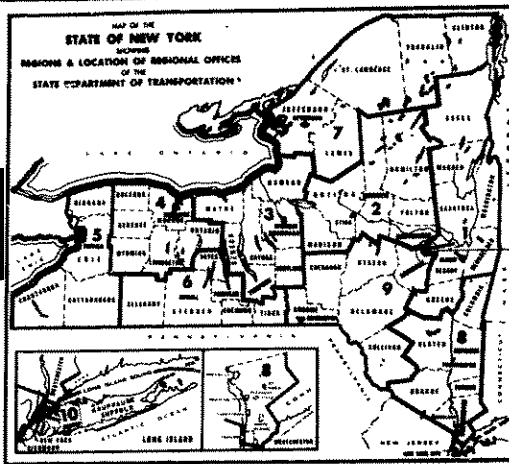
HWY19FH001

(42 pages)



D95749

D95749




STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DESIGN AND CONSTRUCTION DIVISION

PLANS FOR CONSTRUCTING THE
INTERSTATE ROUTE 508
(Central Bridge to Schenectady County Line) SH 78-9
 From Station EB 2275+00 to EB 38+00, a length of 1.86 miles of which 1.66 miles are in the Town of Schoharie
 and 0.20 mile in the Town of Duaneburg
AND RECONSTRUCTING A PORTION OF THE
OAKHILL STREET, SH 9298
 From Station -1+00 to Station 62+00, a length of 1.19 miles in the Town of Esperance
A TOTAL CONTRACT LENGTH OF 3.05 MILES
F.A. PROJECT NO. I-88-1(52)

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	I-88-1(52)	1	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE-SCHENECTADY COUNTY LINE				
SCHOHARIE COUNTY				

TYPE OF CONSTRUCTION
 Unreinforced Cement Conc. Pavement 1.80 miles
 Asphalt Concrete Pavement 1.14 miles
 including
 HGS Relocated Rt. 30 over Interstate 2 Span Composite Girder,
 Total Length 289.50'

STANDARD SHEETS
 203-1, 203-2R1, 203-3, 203-4R1, 203-5R1, 502-2R1, 502-3,
 502-4, 502-5, 502-6, 502-7R1, 502-8, 502-9, 603-1, 603-3,
 606-1R1, 606-2R1, 606-3R1, 607-2, 609-1, 611-1, 625-1,
 645-7, 645-8R1, 645-9, 645-10, 645-11, 645-13R1, 645-14R2,
 646-1, 646-2, 646-3R1, 655-3, 655-8R3, 619-4

PROJECT LOCATION:
 THIS CONTRACT IS A PORTION OF INTERSTATE ROUTE 88, THE SUSQUEHANNA EXPRESSWAY WHICH CONNECTS THE METROPOLITAN AREA OF RINGHAMTON TO THE CAPITAL DISTRICT. THIS PROJECT BEGINS APPROXIMATELY 3500 FEET NORTHEAST OF ROUTE 30A. IT RUNS IN AN EASTERLY DIRECTION FOR 1.80 MILES ON NEW LOCATION, ABOUT 1000+ FEET SOUTH OF ROUTE 2, TO A POINT 950 FEET EAST OF THE SCHOHARIE-SCHENECTADY COUNTY LINE. THIS CONTRACT ALSO PROVIDES FOR THE CONSTRUCTION OF 1.14 MILES OF ROUTE 30 ALSO ON NEW LOCATION IN SCHOHARIE COUNTY.

99 SHEETS CONTRACT NO. D95749
 SCHOHARIE AND SCHENECTADY COUNTIES

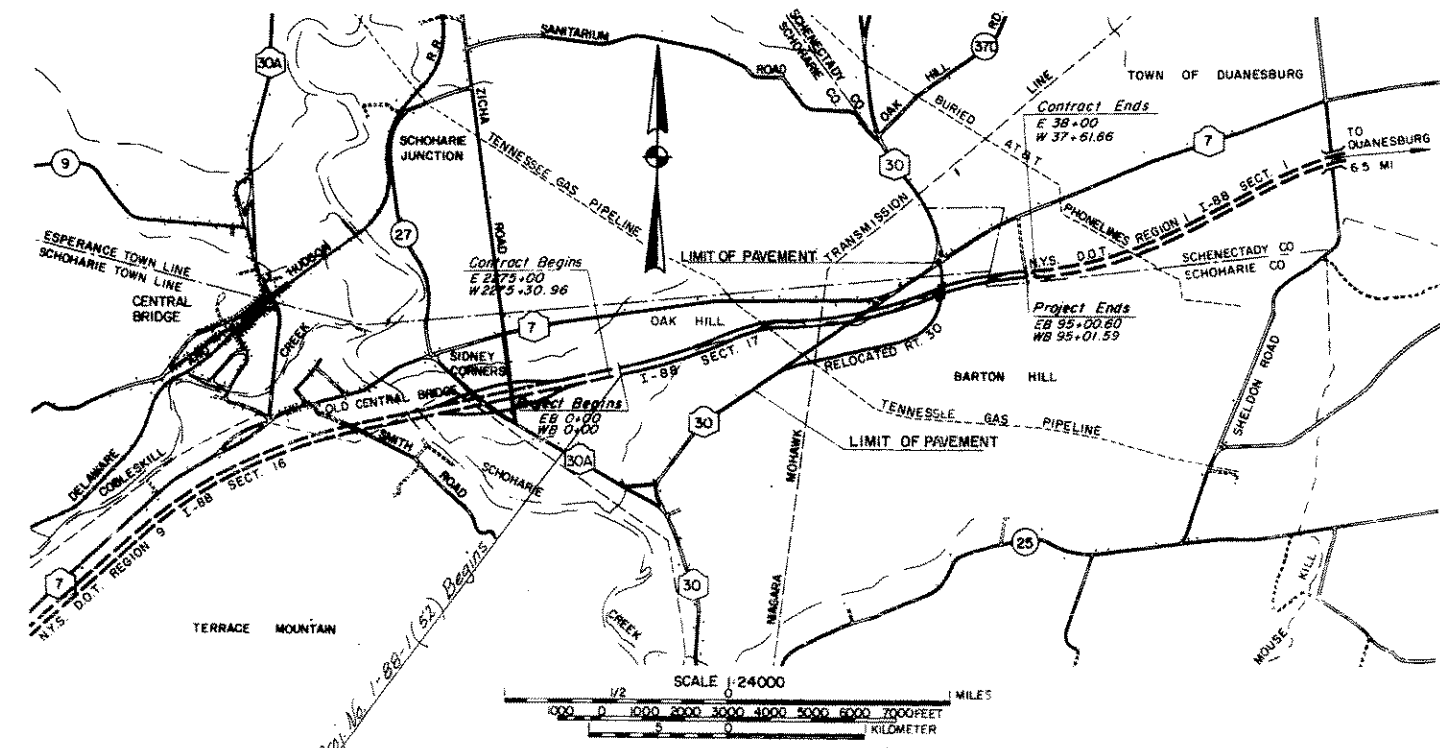
DESIGN DATA:

DESIGN CLASS	MAINLINE		ROUTE 30	
	ALL	ACT	ALL	ACT
CURVE	R-2	R-6M	R-6M	R-6M
MAX GRADE	3%	4%	4%	4%
MIN SSD	500	785	475	495
DESIGN SPEED	70	70	60	60
ESTIMATED 1998 TRAFFIC				
0 HV - 1 WAY	2000	800	250	200
A.A.D.T. - 2 WAY		10,000		2,000

* NEEDED DUE TO MOUNTAINOUS TOPOGRAPHY

NOTES:
 THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THESE HIGHWAYS WILL BE DESIGNATED AS RESTRICTED HIGHWAYS UNDER THIS CONTRACT.
 THE CONTRACTOR WILL BE REQUIRED TO FURNISH WATCHMAN SERVICE UNDER ITEM 619.12 IN ACCORDANCE WITH REQUIREMENT "C" OF THE N.Y.S.D.O.T. SPECIFICATIONS.

AS-BUILTS LOST IN FIRE



CONTRACTOR'S NAME _____
 AWARD DATE _____
 COMPLETION DATE _____
 FINAL ACCEPTANCE DATE _____
 REGIONAL DIRECTOR Name _____
 ENGINEER IN CHARGE Name _____
 FINAL COST TOTAL _____
 FISCAL SHARE COST(S) _____
 PREPARED BY NYS DEPARTMENT OF TRANSPORTATION DESIGN BUREAU

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED _____ DATE _____

DIVISION ADMINISTRATOR _____

NEW YORK STATE DEPARTMENT OF TRANSPORTATION

Approved _____ Chief Engineer

L.W. HALLENBECK _____

Approved _____ Deputy Chief Engineer

Approved _____ Deputy Chief Engineer

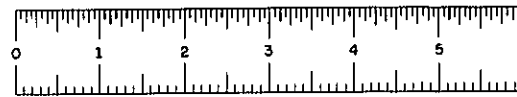
E.V. BOURGAIN _____

FED. ROAD REG. NO.	STATE	DWG. NO.	TOTAL SHEETS
1	N.Y.	G-1	
INTERSTATE ROUTE 508			
CENTRAL BRIDGE TO SCHENECTADY CO. LINE			
S.H. 9298 - SCHOHARIE TO ESPERANCE			
SCHOHARIE - SCHENECTADY COUNTIES			
FEDERAL AID PROJECT NO.	I-88-1(52)		
CAPITAL PROJECT IDENTIFICATION NO.	9357-17-311		

IN CHARGE _____ DATE 9-20-77
 RECOMMENDED BY _____ DATE 12/5/77
 RECOMMENDED BY _____ DATE 12/6-77
 RECOMMENDED BY _____ DATE 12-6-77
 RECOMMENDED BY _____ DATE 8 Dec 77

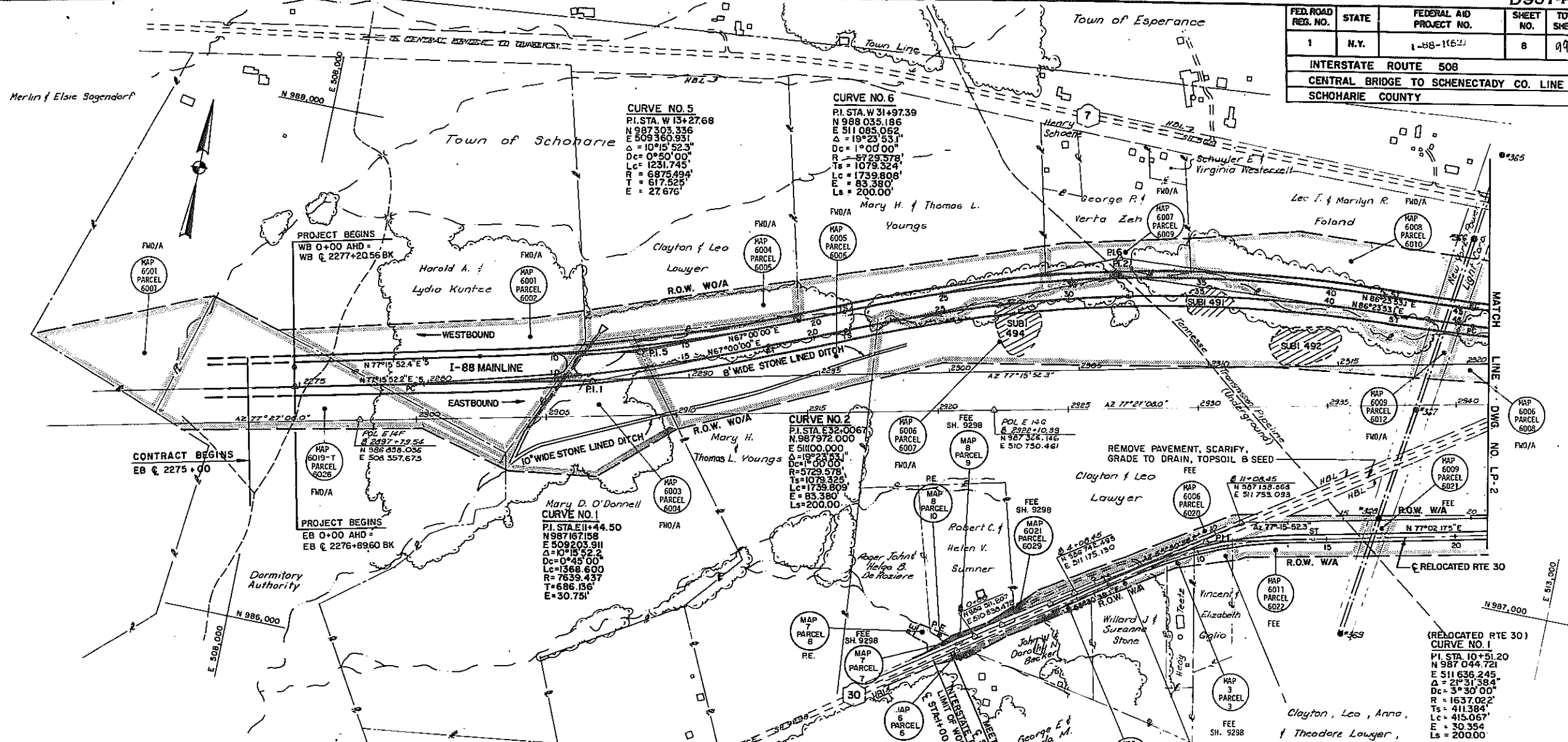
D95749

D95749



D95749

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-58-116(2)	6	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE TO SCHENECTADY CO. LINE				
SCHOHARIE COUNTY				



INTERSTATE ROUTE 508-SECTION 17-CENTRAL BRIDGE TO SCHENECTADY COUNTY LINE			
MAP	PARCEL	TYPE	REPUTED OWNER
6001	6001	FWD/A	HAROLD A. & LYDIA KUNTZ
6001	6002	FWD/A	HAROLD A. & LYDIA KUNTZ
6003	6004	FWD/A	MARY D. O'DONNELL
6004	6005	FWD/A	CLAYTON & LEO LAWYER
6005	6006	FWD/A	MARY H. & THOMAS L. YOUNGS
6006	6007	FWD/A	CLAYTON & LEO LAWYER
6006	6008	FWD/A	CLAYTON & LEO LAWYER
6007	6009	FWD/A	GEORGE P. & VERTA ZEH
6008	6010	FWD/A	LED I. & MARILYN R. FOLAND
6008	6011	FWD/A	LED I. & MARILYN R. FOLAND
6009	6012	FWD/A	NEW YORK POWER & LIGHT CORP.
6010	6013	FWD/A	WILLIAM H. & ETHEL CHRISTIAN
6011	6014	FWD/A	CLAYTON, LEO, ANNA & THEODORE LAWYER AND ROSE DEER
6012	6015	FWD/A	WILLIAM AND LOIS D. WHITNEY
6013	6016	FWD/A	KENNETH I. & MYRTLE A. COONS
6014	6017	FWD/A	KEY DEVELOPMENT CORP.

STATE HWY. 9298-SCHOHARIE TO ESPERANCE			
MAP	PARCEL	TYPE	REPUTED OWNER
6015	6018	FWD/A	CHARLES H. DIETZ
6016	6019	FWD/A	PEOPLE OF N.Y.S. S.H. 9298 ROAD BED
6017	6024	FWD/A	ROBERT V. COONS
6018	6025	A. E.	CLAYTON, LEO, ANNA & THEODORE LAWYER AND ROSE DEER
6019-T	6026	FWD/A	DORMITORY AUTHORITY
6013	6030	P. E.	KENNETH I. & MYRTLE A. COONS

STATE HWY. 9298-SCHOHARIE TO ESPERANCE			
MAP	PARCEL	TYPE	REPUTED OWNER
1	1	FEE	JOHN W. & DOROTHY H. BECKER
2	2	FEE	WILLARD J. & SUZANNE STONE
3	3	FEE	HEDY TEEZT
4	4	FEE	VINCENT & ELIZABETH GIGLIO
6	6	FEE	GEORGE E. & WANDA H. KEYSER

STATE HWY. 9298-SCHOHARIE TO ESPERANCE			
MAP	PARCEL	TYPE	REPUTED OWNER
7	7	FEE	ROGER JOHN & HELGA B. DE ROZIERE
7	8	P.E.	ROGER JOHN & HELGA B. DE ROZIERE
8	9	FEE	ROBERT C. & HELEN V. SUMNER
8	10	P.E.	ROBERT C. & HELEN V. SUMNER
6006	6020	FEE	CLAYTON & LEO LAWYER
6009	6021	FEE	NEW YORK POWER & LIGHT CORP.
6011	6022	FEE	CLAYTON, LEO, ANNA & THEODORE LAWYER AND ROSE DEER
6021	6029	FEE	CLAYTON & LEO LAWYER
6011	6023	FEE	CLAYTON, LEO, ANNA & THEODORE LAWYER AND ROSE DEER
5	5	FEE	WALTER MARTIN JR.
6020	A	T. O.	CLAYTON, LEO, ANNA & THEODORE LAWYER AND ROSE DEER
6020	B	T. O.	CLAYTON, LEO, ANNA & THEODORE LAWYER AND ROSE DEER

NOTE:
THE FOLLOWING SITES HAVE BEEN DESIGNATED AS ARCHAEOLOGICAL AREAS BY THE STATE EDUCATION DEPARTMENT. THESE AREAS ARE SHOWN ON DRAWINGS LP 1 AND LP 2. THE CONTRACTOR WILL NOT WORK IN OR OTHERWISE DISTURB THE PORTIONS OF THESE AREAS WHICH ARE WITHIN THE RIGHT OF WAY BUT OUTSIDE THE WORK LIMITS. THE SITES ARE AS FOLLOWS:

- SUB I 491 SUB I 494
- SUB I 492 SUB I 572

1" = 200' SCALE PLAN			
STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION			
DATE	SCALE	DRAWING NO.	REGION NO.
11/77	1" = 200'	LP-1	9

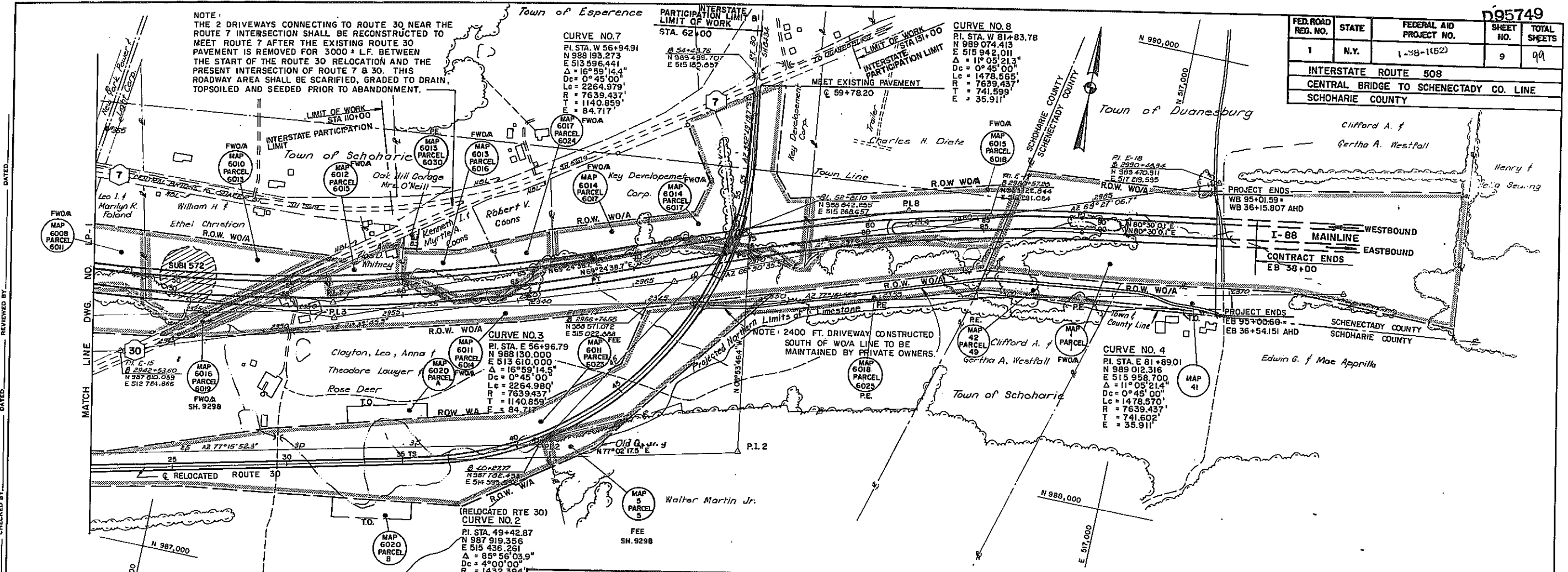
DESIGNED BY: _____ CHECKED BY: _____ DATE: _____ REVIEWED BY: _____ DATE: _____



D95749

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-58-(152)	9	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE TO SCHENECTADY CO. LINE				
SCHOHARIE COUNTY				

NOTE:
THE 2 DRIVEWAYS CONNECTING TO ROUTE 30 NEAR THE ROUTE 7 INTERSECTION SHALL BE RECONSTRUCTED TO MEET ROUTE 7 AFTER THE EXISTING ROUTE 30 PAVEMENT IS REMOVED FOR 3000' L.F. BETWEEN THE START OF THE ROUTE 30 RELOCATION AND THE PRESENT INTERSECTION OF ROUTE 7 & 30. THIS ROADWAY AREA SHALL BE SCARIFIED, GRADED TO DRAIN, TOPSOILED AND SEEDED PRIOR TO ABANDONMENT.



ITEM 625.01, CONCRETE RIGHT-OF-WAY MARKERS, TYPE L (LOW) EACH

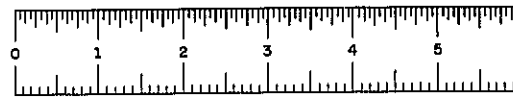
FROM I-88 MAINLINE BASELINE			FROM I-88 MAINLINE BASELINE			FROM I-88 MAINLINE BASELINE			FROM S.H. 9298 BASELINE			FROM S.H. 9298 BASELINE		
MON. NO.	BASELINE STA.	OFFSET	MON. NO.	BASELINE STA.	OFFSET	MON. NO.	BASELINE STA.	OFFSET	MON. NO.	BASELINE STA.	OFFSET	MON. NO.	BASELINE STA.	OFFSET
1	2895+35	31± RT	23	2978+84	294' RT	45	2957+75.93	250.7± LT	54	0-186	9± LT	75	57+24	280± LT
2	2900+25.12	37.4' RT	24	2979+60	305± RT	46	2961+83	285± LT	55	0-173	47' LT	76	0-195	39± RT
3	2903+43	199± RT	25	2983+38	405± RT	47	2964+85	285± LT	56	0-211	94' LT	77	0-61	63' RT
4	2907+00	225' RT	26	2985+15	445± RT	48	2973+14	235± LT	57	0-171	125' LT	78	0+95	51' RT
5	2909+94.46	51± RT	27	2894+53	360± LT	49	2977+17	170± LT	58	0-94	32' LT	79	4+07	54± RT
6	2915+08	65± LT	28	2900+39	371± LT	50	2982+15.00	108.1' LT	59	0+73	22' LT	80	8+02	60' RT
7	2920+14.51	180.7' LT	29	2904+4372	379.6' LT	51	2982+46	156± LT	60	3+04.16	67.4± LT	81	10+23.7	122.6' RT
8	2924+39.33	170± LT	30	2908+53	428± LT	52	2985+85	71± LT	61	6+99.16	55.7' LT	82	10+73.37	128.6' RT
9	2928+64.15	161± LT	31	2912+62	481± LT	53	2989+25	18± RT	62	10+94.16	44.2' LT	83	15+64.5	136.5' RT
10	2932+88.97	153.2' LT	32	2916+72	531± LT				63	12+91	25± LT	84	20+50	142± RT
11	2938+58.63	103± LT	33	2920+81	581± LT				64	16+19.1	17.6± LT	85	24+90.37	145.1' RT
12	2943+97.16	42± LT	34	2924+90.22	631.0' LT				65	21+53.37	5.9± LT	86	29+00	181± RT
13	2945+03.23	21± LT	35	2929+47.5	652± LT				66	24+90.37	48.9' LT	87	32+65	217± RT
14	2950+15	18± RT	36	2934+04.77	673.4 LT				67	28+52.21	95.1' LT	88	36+91.56	255.6' RT
15	2955+25	57± RT	37	2937+59	616± LT				68	33+05	115± LT	89	39+75	144± RT
16	2959+80.16	89.8' RT	38	2941+12.99	560± LT				69	36+91.56	131± LT	90	41+68.16	183.8' RT
17	2975+32	178± RT	39	2945+18	480± LT				70	39+84.46	145.7' LT	91	45+86	222± RT
18	2979+86	223± RT	40	2948+64	388± LT				71	45+17.07	9.7' LT	92	51+85.28	279.4' RT
19	2985+73	441± RT	41	2952+08.74	295± LT				72	47+58.88	75.1' LT	93	54+30.30	179.7' RT
20	2989+25	441± RT	42	2953+47	284± LT				73	53+50.03	215.3' LT	94	55+31.63	126± RT
21	2969+64	202± RT	43	2955+93	363± LT				74	54+94.10	161.2' LT	95	59+56	126± RT
22	2974+23	245± RT	44	2955+84.2	274.2± LT									

1" = 200' SCALE PLAN

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION

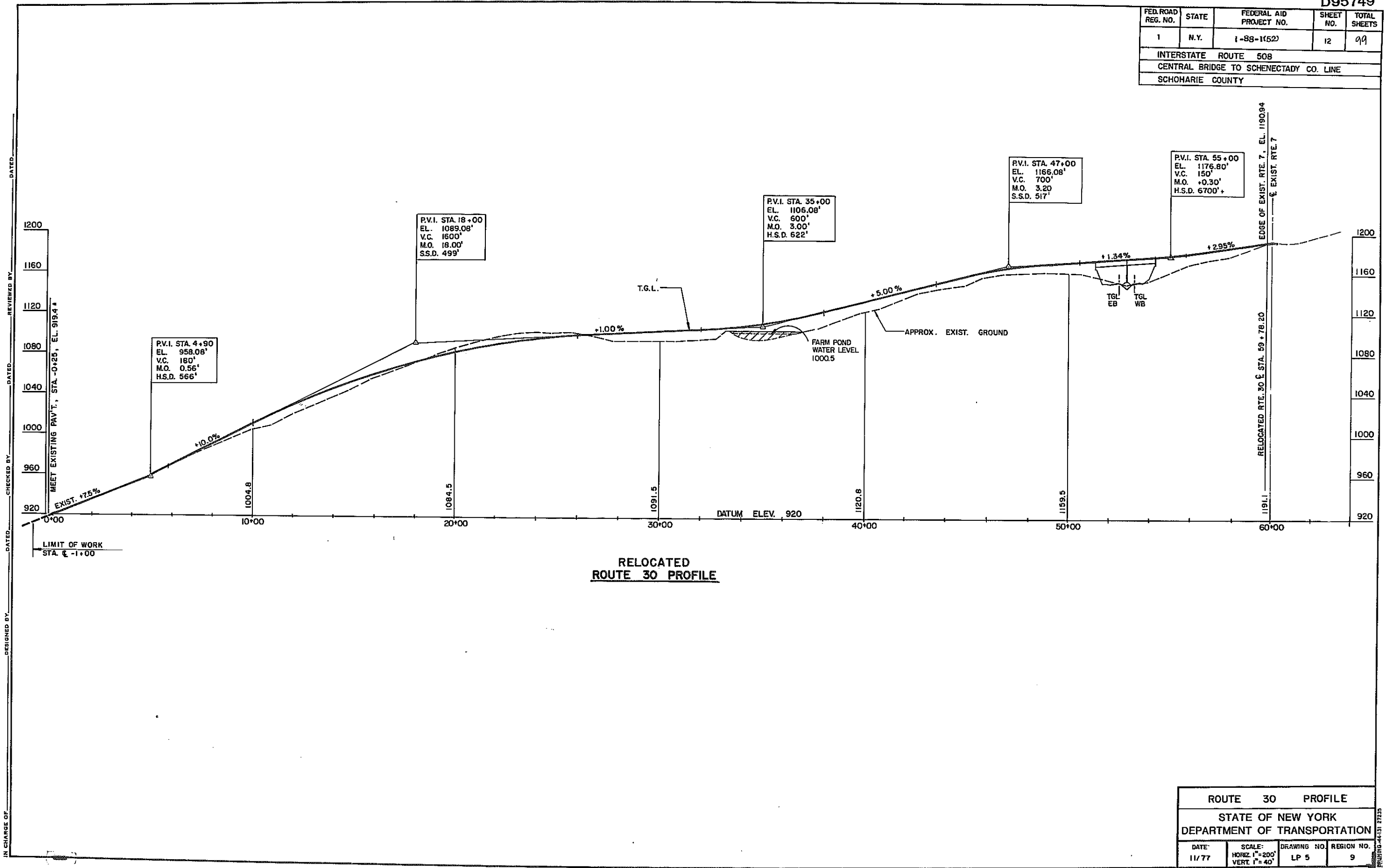
DATE	SCALE	DRAWING NO.	REGION NO.
11/77	1" = 200'	LP-2	9

DESIGNED BY: _____ CHECKED BY: _____ DATED: _____
 DRAWN BY: _____ REVIEWED BY: _____ DATED: _____
 IN CHARGE OF: _____



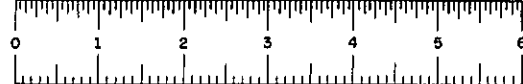
D95749

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-88-1(52)	12	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE TO SCHENECTADY CO. LINE				
SCHOHARIE COUNTY				



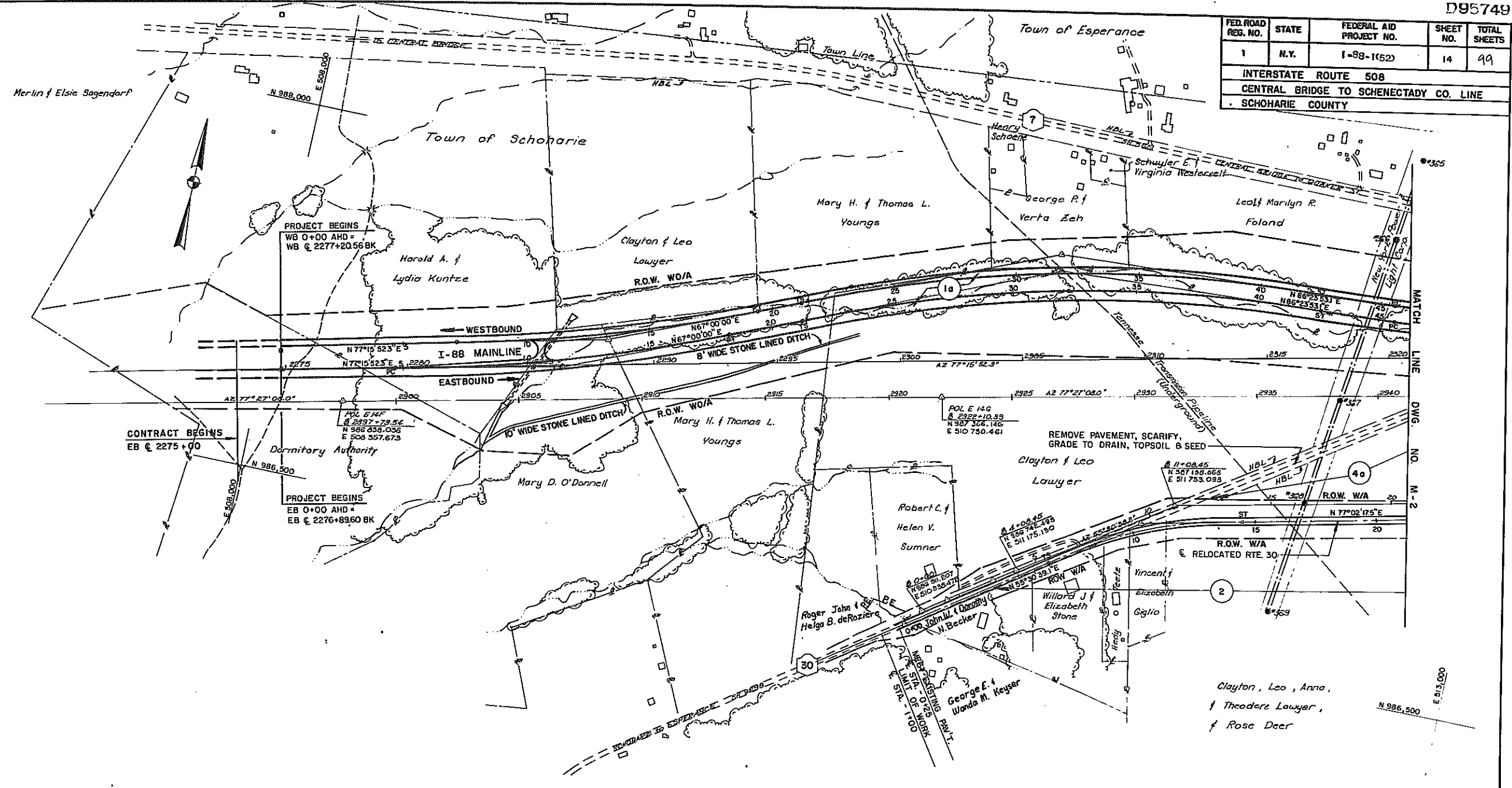
DESIGNED BY _____ DATED _____ CHECKED BY _____ DATED _____ REVIEWED BY _____ DATED _____ IN CHARGE OF _____

ROUTE 30 PROFILE			
STATE OF NEW YORK			
DEPARTMENT OF TRANSPORTATION			
DATE: 11/77	SCALE: HORIZ. 1"=200' VERT. 1"=40'	DRAWING NO. LP 5	REGION NO. 9



D95749

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	I-88-1(52)	14	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE TO SCHENECTADY CO. LINE				
SCHOHARIE COUNTY				



CIRCLED NUMBERS ON THE PLAN REFER TO CORRESPONDING PART NUMBERS IN THE TABLE OF MAINTENANCE ON DWG. NO. M-2.

1"=200' MAINT. PLAN			
STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION			
DATE	SCALE	DRAWING NO.	REGION NO.
11/77	1" = 200'	M-1	9

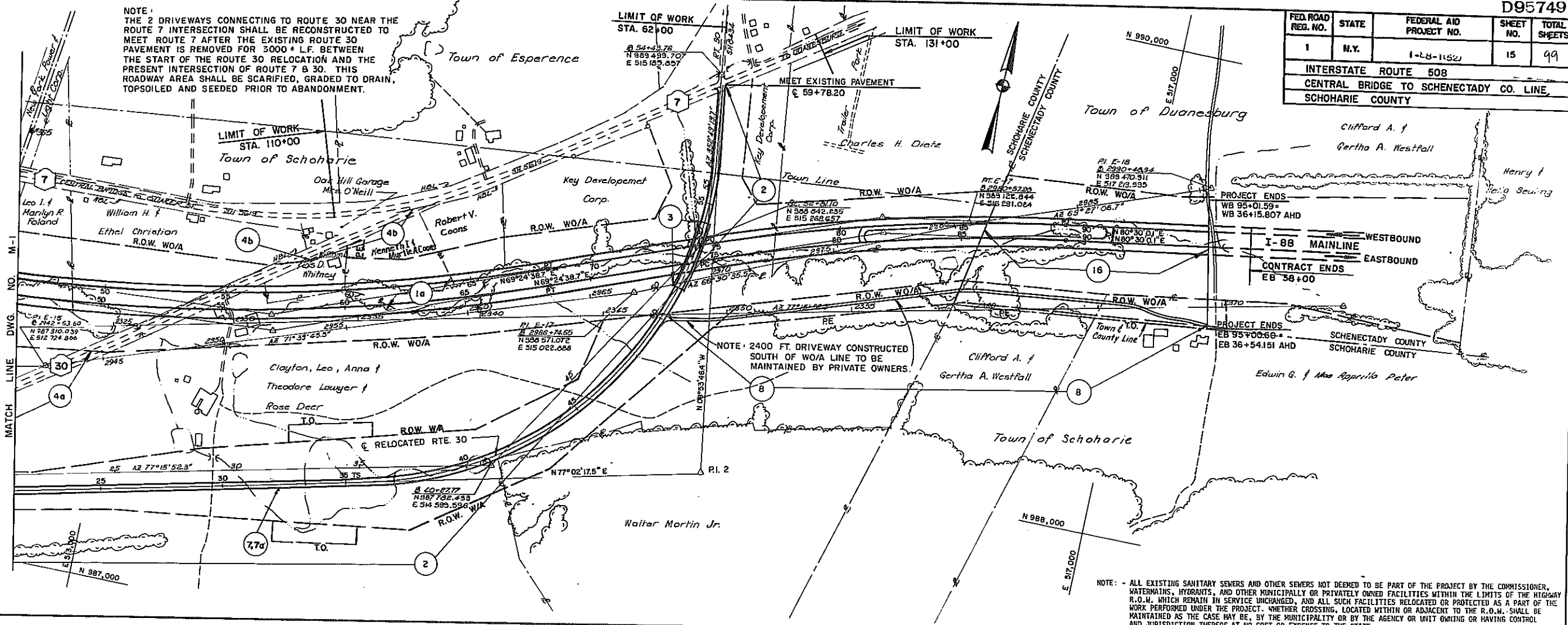
DESIGNED BY _____ CHECKED BY _____ DATED _____ REVIEWED BY _____ DATED _____



D95749

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	I-88-1152J	15	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE TO SCHENECTADY CO. LINE				
SCHOHARIE COUNTY				

NOTE: THE 2 DRIVEWAYS CONNECTING TO ROUTE 30 NEAR THE ROUTE 7 INTERSECTION SHALL BE RECONSTRUCTED TO MEET ROUTE 7 AFTER THE EXISTING ROUTE 30 PAVEMENT IS REMOVED FOR 3000' L.F. BETWEEN THE START OF THE ROUTE 30 RELOCATION AND THE PRESENT INTERSECTION OF ROUTE 7 & 30. THIS ROADWAY AREA SHALL BE SCARIFIED, GRADED TO DRAIN, TOPSOILED AND SEEDED PRIOR TO ABANDONMENT.



PROJECT ENDS
WB 95+01.59+
WB 36+15.807 AHD

I-88 MAINLINE
WESTBOUND
EASTBOUND

CONTRACT ENDS
EB 58+00

PROJECT ENDS
EB 95+00.68+
EB 36+54.151 AHD

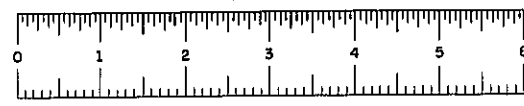
NOTE: - ALL EXISTING SANITARY SEWERS AND OTHER SEWERS NOT DEEMED TO BE PART OF THE PROJECT BY THE COMMISSIONER, WATERMANS, HYDRANTS, AND OTHER MUNICIPALLY OR PRIVATELY OWNED FACILITIES WITHIN THE LIMITS OF THE HIGHWAY R.O.W. WHICH REMAIN IN SERVICE UNCHANGED, AND ALL SUCH FACILITIES RELOCATED OR PROTECTED AS A PART OF THE WORK PERFORMED UNDER THE PROJECT, WHETHER CROSSING, LOCATED WITHIN OR ADJACENT TO THE R.O.W. SHALL BE MAINTAINED AS THE CASE MAY BE, BY THE MUNICIPALITY OR BY THE AGENCY OR UNIT OWNING OR HAVING CONTROL AND JURISDICTION THEREOF AT NO COST OR EXPENSE TO THE STATE.

TABLE OF MAINTENANCE

PART NO.	HIGHWAY	LIMITS	FEATURES TO BE MAINTAINED	FILES	LANE MILES	AGENCY	JURISDICTION	PART NO.	HIGHWAY	LIMITS	FEATURE TO BE MAINTENANCE	FILES	LANE MILES	AGENCY	JURISDICTION
INTERSTATE HIGHWAYS															
1a	M I-508 SECT. 17 CENTRAL BRIDGE TO SCHENECTADY CO. LINE	EB 0+00 TO EB 85+81 WB 0+00 TO WB 85+05	PAVEMENT, SHOULDERS, DRAINAGE, LANDSCAPING	1.627	4.881	N.Y.S.D.O.T. SCHOHARIE CO. RES.	SECTION 340-b OF HIGHWAY LAW	5a.	M I-508 SECT. 17 CENTRAL BRIDGE TO SCHENECTADY CO. LINE (SCHOHARIE CO)	EB 0+00 TO EB 85+81 WB 0+00 TO WB 85+05	PAVEMENT OF PART 1a	1.627	4.881	N.Y.S.D.O.T. SCHOHARIE CO. RES.	SECTION 340-b OF HIGHWAY LAW
1b	M I-508 SECT. 17 SCHENECTADY CO. LINE TO END OF PROJECT	EB 85+81 TO EB 95+00.60 WB 85+05 TO WB 95+01.89	PAVEMENT, SHOULDERS, DRAINAGE, LANDSCAPING	.172	.344	N.Y.S.D.O.T. SCHEN. CO. RES.	SECTION 340-b OF HIGHWAY LAW	5b.	M I-508 SECTION 17 SCHENECTADY CO. LINE TO END OF PROJECT (SCHENECTADY CO.)	EB 85+81 TO EB 95+00.60 WB 85+05 TO WB 95 TO 1.89	PAVEMENT OF PART 1b	.172	.344	N.Y.S.D.O.T. SCHENECTADY CO.	SECTION 340-b OF HIGHWAY LAW
STATE HIGHWAYS															
2.	SH 9298 RTE 30 RELOCATED PORTION	E -0+25 TO E 59+78.20	PAVEMENT, SHOULDERS, DRAINAGE, LANDSCAPING	1.137	2.274	N.Y.S.D.O.T.	SECTION 12 OF HIGHWAY LAW	6.	RELOCATED SH 9298 (ROUTE 30)	E -0+25 TO E 59+78.20	PAVEMENT OF PART 2,3	1.137	2.274	N.Y.S.D.O.T. SCHOHARIE CO. RES.	SECTION 12 OF HIGHWAY LAW
BRIDGES															
3.	SH 9298 BRIDGE OVER MAINLINE I-508	E 51+82.84 TO E 54+75.51	ENTIRE STRUCTURE INCLUDING PAVEMENT, SIDEWALKS, CURBS AND RAILINGS	.055	.111	N.Y.S.D.O.T.	SECTION 340-b OF HIGHWAY LAW	7	RELOCATED RTE 30 CATTLE PASS	E 32+20	STRUCTURAL INTEGRITY	-	-	N.Y.S.D.O.T.	SECTION 12 OF HIGHWAY LAW
ROADS, ABANDONED OR DESTROYED BY CONSTRUCTION															
4a.	SH 9298 RTE 30 ABANDON PORTION (SOUTH OF R)	B 11+23+35' LT TO B 24+20 490' LT	ABANDON AND REMOVE RIGHT OF WAY TO REVERT TO TOWN OF SCHOHARIE	.336	.700	ABANDON TO LOCAL JURISDICTION TOWN OF SCHOHARIE	SECTION 62 OF HIGHWAY LAW	7a.	RELOCATED RTE 30 CATTLE PASS	E 32+20	CLEANING OF UNDERPASS	-	-	ADJACENT PROPERTY OWNER	AGREEMENT DATED
4b.	SH 9298 RTE 30 ABANDON PORTION (NORTH OF R)	B 32+30 840' LT TO OLD INTERSECTION @ RTE 7	ABANDON AND REMOVE RIGHT OF WAY TO REVERT TO TOWN OF SCHOHARIE	.014	.028	" " "	" " "	8.	12' COMMON ACCESS DRIVE	STA. 0+24 TO STA. 24+00	PAVEMENT, SHOULDERS, DRAINAGE, LANDSCAPING AND SNOW REMOVAL	.45	.45	ADJACENT PROPERTY OWNERS	SECTION 54-a OF HIGHWAY LAW

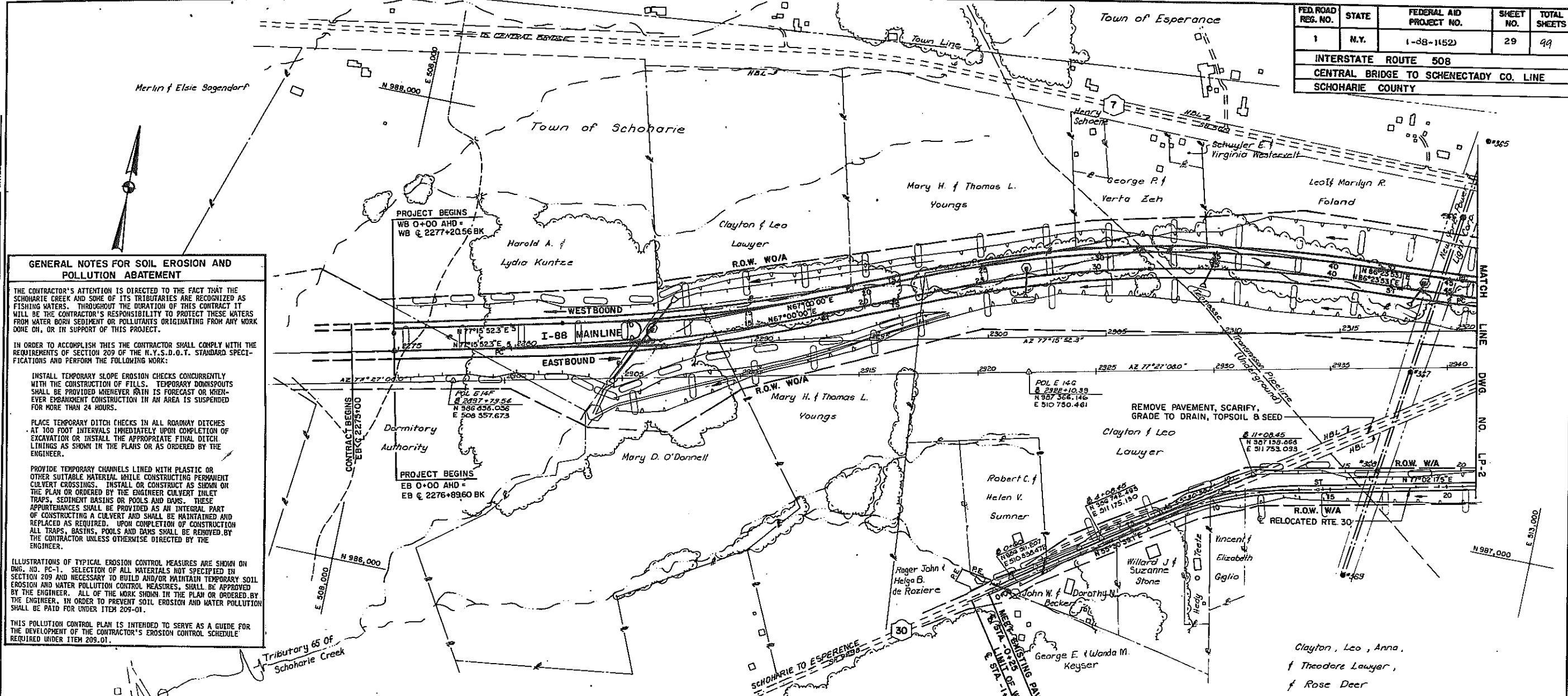
CIRCLED NUMBERS ON THE PLAN REFER TO CORRESPONDING PART NUMBERS IN THE TABLE OF MAINTENANCE.

MAINT. JURISDICTION TABLE			
1" = 200' MAINT. PLAN			
STATE OF NEW YORK			
DEPARTMENT OF TRANSPORTATION			
DATE	SCALE	DRAWING NO.	REGION NO.
11/77	1" = 200'	M-2	9



D95749

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-38-1152	29	49
INTERSTATE ROUTE 508				
CENTRAL BRIDGE TO SCHENECTADY CO. LINE				
SCHOHARIE COUNTY				



GENERAL NOTES FOR SOIL EROSION AND POLLUTION ABATEMENT

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THE SCHOHARIE CREEK AND SOME OF ITS TRIBUTARIES ARE RECOGNIZED AS FISHING WATERS. THROUGHOUT THE DURATION OF THIS CONTRACT IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT THESE WATERS FROM WATER BORN SEDIMENT OR POLLUTANTS ORIGINATING FROM ANY WORK DONE ON, OR IN SUPPORT OF THIS PROJECT.

IN ORDER TO ACCOMPLISH THIS THE CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF SECTION 209 OF THE N.Y.S.D.O.T. STANDARD SPECIFICATIONS AND PERFORM THE FOLLOWING WORK:

INSTALL TEMPORARY SLOPE EROSION CHECKS CONCURRENTLY WITH THE CONSTRUCTION OF FILLS. TEMPORARY DOWNSPOUTS SHALL BE PROVIDED WHENEVER RAIN IS FORECAST OR WHENEVER EMBANKMENT CONSTRUCTION IN AN AREA IS SUSPENDED FOR MORE THAN 24 HOURS.

PLACE TEMPORARY DITCH CHECKS IN ALL ROADWAY DITCHES AT 100 FOOT INTERVALS IMMEDIATELY UPON COMPLETION OF EXCAVATION OR INSTALL THE APPROPRIATE FINAL DITCH LININGS AS SHOWN IN THE PLANS OR AS ORDERED BY THE ENGINEER.

PROVIDE TEMPORARY CHANNELS LINED WITH PLASTIC OR OTHER SUITABLE MATERIAL WHILE CONSTRUCTING PERMANENT CULVERT CROSSINGS. INSTALL OR CONSTRUCT AS SHOWN ON THE PLAN OR ORDERED BY THE ENGINEER CULVERT INLET TRAPS, SEDIMENT BASINS OR POOLS AND DAMS. THESE APPURTENANCES SHALL BE PROVIDED AS AN INTEGRAL PART OF CONSTRUCTING A CULVERT AND SHALL BE MAINTAINED AND REPLACED AS REQUIRED. UPON COMPLETION OF CONSTRUCTION ALL TRAPS, BASINS, POOLS AND DAMS SHALL BE REMOVED BY THE CONTRACTOR UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

ILLUSTRATIONS OF TYPICAL EROSION CONTROL MEASURES ARE SHOWN ON DWG. NO. PC-1. SELECTION OF ALL MATERIALS NOT SPECIFIED IN SECTION 209 AND NECESSARY TO BUILD AND/OR MAINTAIN TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL MEASURES, SHALL BE APPROVED BY THE ENGINEER. ALL OF THE WORK SHOWN IN THE PLAN OR ORDERED BY THE ENGINEER, IN ORDER TO PREVENT SOIL EROSION AND WATER POLLUTION SHALL BE PAID FOR UNDER ITEM 209-01.

THIS POLLUTION CONTROL PLAN IS INTENDED TO SERVE AS A GUIDE FOR THE DEVELOPMENT OF THE CONTRACTOR'S EROSION CONTROL SCHEDULE REQUIRED UNDER ITEM 209-01.

GUIDELINES FOR SOIL EROSION AND POLLUTION ABATEMENT

IN ADDITION TO PERFORMING THE WORK REQUIRED UNDER ITEM 209-01, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLANNING, SCHEDULING AND CONDUCTING HIS CONSTRUCTION OPERATION IN A MANNER WHICH WILL CONTRIBUTE TO THE OBJECTIVE OF SOIL EROSION AND POLLUTION ABATEMENT. ON THIS PROJECT THE CONTRACTOR SHALL CONDUCT HIS CONSTRUCTION OPERATIONS IN ACCORDANCE WITH THE FOLLOWING GUIDELINES DEVELOPED WITH THE OBJECTIVE OF REDUCING SOIL EROSION AND WATER POLLUTION BY CONTROLLING RUNOFF THROUGH THE PROJECT.

STA. 0+00 TO STA. 24+00±

THE INDEPENDENT DITCH SOUTH OF THE MAINLINE BETWEEN STATIONS 10 AND 24 SHALL BE COMPLETELY CONSTRUCTED AT AN EARLY STAGE OF CONSTRUCTION. THE EXCAVATED MATERIAL FROM THIS DITCH MAY BE USED IN EMBANKMENTS BETWEEN STATIONS 0+00 AND 10+00. UPON COMPLETION OF THE DITCH AND ITS LINING, UPLAND RUNOFF SHALL BE DIVERTED INTO IT, AND EMBANKMENT CONSTRUCTION BETWEEN STATION 10 AND 24 MAY BE PERMITTED.

STA. 25+00± TO STA. 54+00± (WEST OF EXISTING ROUTE 30).

THE CONTRACTOR SHOULD ANTICIPATE THAT HE WILL HAVE TO PROVIDE TEMPORARY DITCHING IN SEVERAL STAGES PRIOR TO BEING ABLE TO PROVIDE FOR RUNOFF IN THE PROPOSED LINED ROADSIDE DITCHES. AS A FIRST STAGE THE CUTS NORTH OF THE EXISTING DITCH SHOULD BE STARTED AND PROGRESSED UNTIL THEY PROVIDE A TEMPORARY DITCH INTO WHICH WATER FROM THE 42" CULVERT UNDER EXISTING ROUTE 30 CAN BE DIVERTED. (THE CLOSING OF EXISTING ROUTE 30 WILL NOT BE PERMITTED UNTIL AFTER TRAFFIC IS ROUTED ONTO RELOCATED ROUTE 30). THE DIVERSION OF RUNOFF TO THE TEMPORARY DITCH NORTH OF THE EXISTING DRAINAGE WAY WILL PERMIT EXCAVATION OF THE CUT SOUTH OF THE DRAINAGE WAY. THESE EXCAVATIONS IN THESE CUTS SHOULD THEN BE PROGRESSED TO COMPLETION AND THE STONE ROADWAY DITCH LINING INSTALLED PRIOR TO PERMITTING RUNOFF FROM THE EXISTING 42" CULVERT TO BE DIRECTED INTO THE SOUTHERLY DITCH LINE (RELOCATION OF THE GAS MAIN CROSSING AT STA. 33± WILL HAVE TO BE EFFECTED BEFORE THE ENTIRE EXCAVATION CAN BE COMPLETED.) THE DIVERSION OF RUNOFF TO THE FINISHED SOUTHERLY ROADWAY DITCH WILL PERMIT RESUMPTION OF THE EXCAVATION AND COMPLETION OF THE CONSTRUCTION OF THE NORTHERLY ROADSIDE DITCH.

STA. 55+00± TO STA. 95+00±

THE CONTRACTOR SHOULD MAKE AN EFFORT TO LEAVE THE RUNOFF RUN GENERALLY IN ITS EXISTING DITCH WHILE HE PROGRESSES THE EXCAVATION FOR BOTH THE NORTHERLY AND SOUTHERLY ROADWAY DITCHES TO COMPLETION. UPON COMPLETION OF THE LININGS FOR THE ROADWAY DITCHES AND THE DIVERSION OF RUNOFF TO THEM, THE REMAINDER OF THE EXCAVATIONS MAY BE PROGRESSED.

THE CONTRACTOR WILL BE PERMITTED TO MODIFY THESE GUIDE LINES FOR THIS PROJECT IN DEVELOPING HIS EROSION CONTROL SCHEDULE PROVIDED IT, IN THE OPINION OF THE ENGINEER IS MORE EFFECTIVE IN ACHIEVING THE OBJECTIVE OF SOIL EROSION AND POLLUTION ABATEMENT.

POLLUTION CONTROL LEGEND

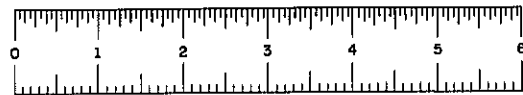
	TEMPORARY CULVERT INLET TRAP
	TEMPORARY DITCH CHECK
	DIRECTION OF FLOW IN DITCH

TEMPORARY POLLUTION CONTROL

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION

DATE	SCALE	DRAWING NO.	REGION NO.
11/77	1" = 200'	PC-2	2

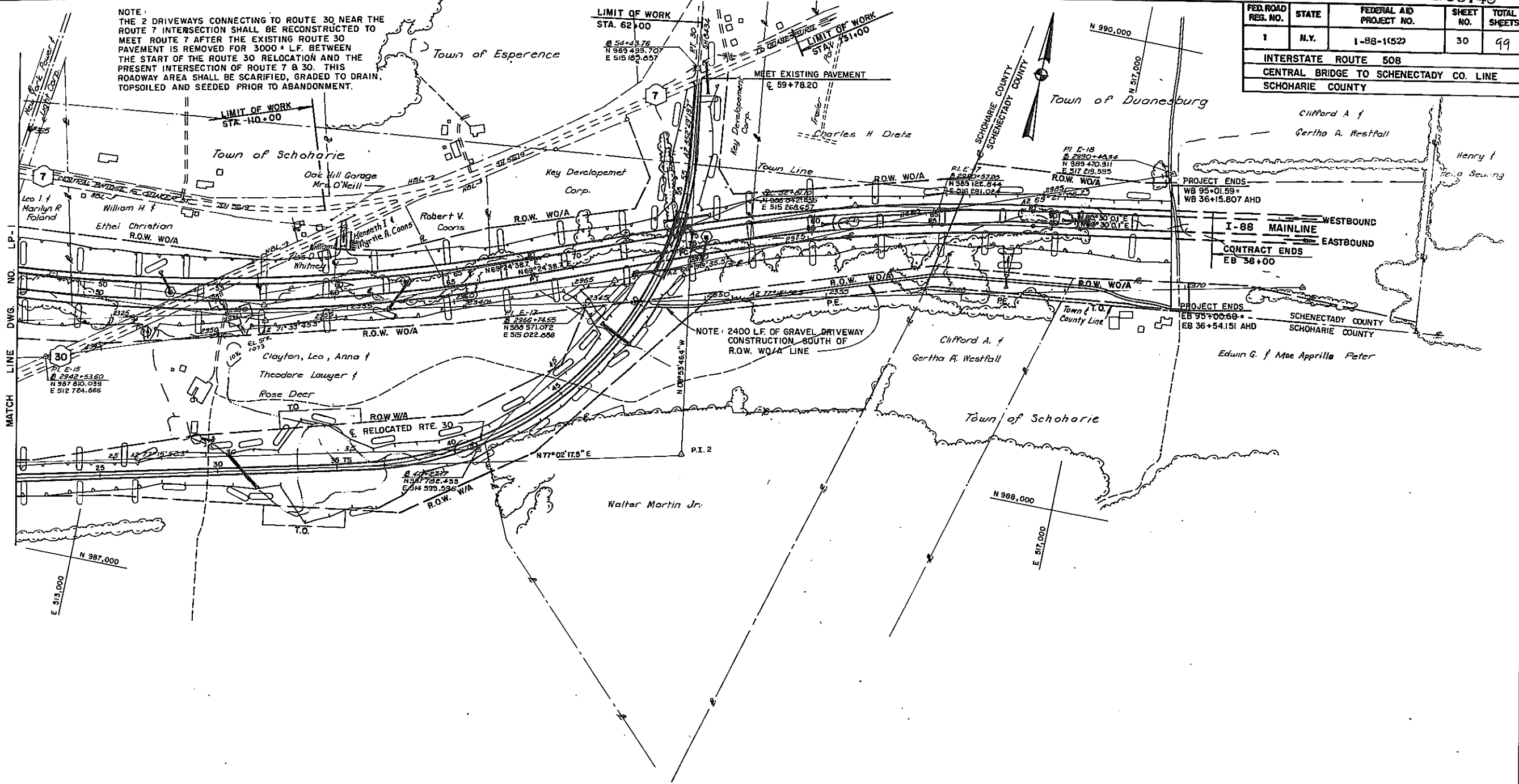
DESIGNED BY: _____ CHECKED BY: _____ DATED: _____ REVIEWED BY: _____ DATED: _____



D95749

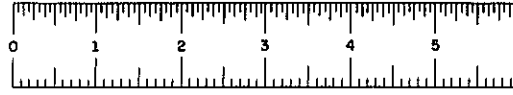
FED. ROAD DIST. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-88-1152	30	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE TO SCHENECTADY CO. LINE				
SCHOHARIE COUNTY				

NOTE:
THE 2 DRIVEWAYS CONNECTING TO ROUTE 30 NEAR THE ROUTE 7 INTERSECTION SHALL BE RECONSTRUCTED TO MEET ROUTE 7 AFTER THE EXISTING ROUTE 30 PAVEMENT IS REMOVED FOR 3000' L.F. BETWEEN THE START OF THE ROUTE 30 RELOCATION AND THE PRESENT INTERSECTION OF ROUTE 7 & 30. THIS ROADWAY AREA SHALL BE SCARIFIED, GRADED TO DRAIN, TOPSOILED AND SEEDED PRIOR TO ABANDONMENT.



DATED _____ REVIEWED BY _____ DATED _____ CHECKED BY _____ DATED _____ DESIGNED BY _____ IN CHARGE OF _____

TEMPORARY POLLUTION CONTROL			
STATE OF NEW YORK			
DEPARTMENT OF TRANSPORTATION			
DATE	SCALE	DRAWING NO.	REGION NO.
11/77	1" = 200'	PC-3	9



D95749

SIGN TEXT DATA SHEET

FED. RD. REG. NO.	STATE	FEDERAL PROJECT	AID NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	(-88-162)		32	99

INTERSTATE ROUTE 508
CENTRAL BRIDGE-SCHENECTADY COUNTY LINE
SCHOHARIE COUNTY

LOCATION OF SIGNS

TEXT NO.	LOCATION NO.	STATION	SIDE
1	1	WB 38 + 20	LT
2	2	EB 85 + 01	RT
3	3	WB 86 + 05	LT
4	4	RT 30 - 57 + 00	RT
6	6	RT 7 - 123 + 00	RT
9	9	RT 7 - 130 + 50	LT
10	10	RT 30 - 64 + 10	LT
21	11	RT 30 - 6 + 00	LT
26	12	RT 30 - 50 + 00	LT
13	13	RT 7 - 122 + 50	RT
14	14	EB 0 + 50	RT
14	15	EB 50 + 00	RT
15	16	EB 9 + 00	RT
16	17	EB 9 + 75	LT
16	18	WB 11 + 40	RT
26	19	EB 17 + 66	RT
26	20	WB 11 + 42	LT
25	21	WB 15 + 00	LT
25	22	RT 30 - 55 + 50	LT
16	23	EB 80 + 50	LT
16	24	EB 90 + 50	LT
16	25	WB 81 + 50	RT
16	26	WB 91 + 50	RT
18	27	RT 30 - 2 - 50	RT
26	28	RT 30 - 55 + 50	RT
20	29	RT 30 - 15 + 00	LT
19	30	RT 30 - 23 + 50	LT
19	31	RT 30 - 26 + 00	RT
22	32	RT 30 - 28 + 50	LT
22	33	RT 30 - 31 + 50	LT
25	34	RT 30 - 50 + 50	RT
28	35	RT 30 - 55 + 00	LT
25	36	RT 30 - 31 + 30	RT
26	37	RT 30 - 33 + 45	RT
25	38	RT 30 - 33 + 60	LT
26	39	RT 30 - 30 + 60	LT
31	40	RT 30 - 54 + 50	RT

LISTING CONTINUED ON NEXT SHEET DNG. NO. S-2

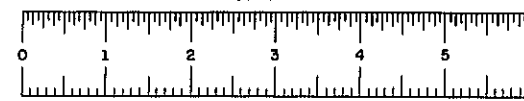
ITEM NO.	LOCATION NO.	TEXT NO.	TEXT	LETTER SIZE	APPROX. SIZE OF SIGN	M.U.T.C.D. NO.	COLOR		TYPE OF MOUNTING
							BACKGROUND	CHARACTERS	
645.06 (SEE NOTE)	1	1	EXIT SCHOHARIE CENTRAL BRIDGE 1 MILE	SEE M.U.T.C.D.	8'6" x 2'6" 22'0" x 14'0" (329.25 SF)	D10-2 D-4	SEE M.U.T.C.D.	SEE M.U.T.C.D. TYPE I OR II	GR. MTD. TYPE 14
645.06	2	2	SCHENECTADY COUNTY		5'6" x 2'6" (13.75 SF)	D-62		TYPE IV	TYPE 2
645.06	3	3	SCHOHARIE COUNTY		5'0" x 2'6" (12.5 SF)	D-62			TYPE (2)
645.06	4	4	ESPERANCE 5 DUANESBURG 8		8'6" x 3'6" (29.75 SF)	D-2			645.30 (2)
645.06			ESPERANCE 5 N.I.C.		8'0" x 2'0" (16.0 SF)	D15A-1			X
645.06	6	6	DUANESBURG 8 ESPERANCE 5		8'0" x 3'6" 28.0 SF	D-2			TYPE 2
645.06			DUANESBURG 8 SCHENECTADY 18 N.I.C.		9'6" x 3'6" (332.5 SF)	D15A-2			X
645.06			CENTRAL BRIDGE 3 COBLESKILL 11		11'0" x 3'6" (38.5 SF)	D15A-2			X
645.06	9	9	COBLESKILL 11 SCHOHARIE 5 ESPERANCE 5		8'0" x 5'0" (40.0 SF.)	D3			645.30 (2)
645.06	10	10	SCHOHARIE 5 DUANESBURG 8 COBLESKILL 11		8'6" x 5'0" (42.5 SF.)	D3			645.30 (2)
645.06			SCHOHARIE 5 N.I.C.		8'0" x 2'0" (16 SF)	D15A-1			X
645.06			TOWN OF ESPERANCE		3'6" x 1'6" (5.25 SF)	D62A			X
645.06	13	13	TOWN OF SCHOHARIE	SEE M.U.T.C.D.	3'0" x 1'6" (45 SF)	D62A	SEE M.U.T.C.D.	SEE M.U.T.C.D. TYPE IX	TYPE I

ITEM NO.	LOCATION NO.	TEXT NO.	TEXT	LETTER SIZE	APPROX. SIZE OF SIGN	M.U.T.C.D. NO.	COLOR		TYPE OF MOUNTING
							BACKGROUND	CHARACTERS	
645.2050	14, 15	14	SLOWER TRAFFIC KEEP RIGHT	SEE M.U.T.C.D.	48" x 60"	R31A-S	SEE M.U.T.C.D.	SEE M.U.T.C.D.	GR
645.2050	16	15	STATE SPEED LIMIT 55		48" x 60"	R8B			GR
645.2090	17, 18, 23, 24, 25, 26	16	NO U-TURN		48" x 48"	R20-C			GR
645.2020	42	17	STOP		30" x 30"	R-2			GR
645.2300	27, 45	18	RIGHT TURN		30" x 30"	W2-R			GR
645.2300	30, 31	19	LEFT TURN		30" x 30"	W2-L			GR
645.2080	29	20	STATE SPEED LIMIT 55		36" x 48"	R8			GR
645.2300	11	21	ROAD NARROWS		30" x 30"	W6I			GR
645.2441	32, 33	22	HILL 1 MILE USE LOW GEAR		48" x 48" 36" x 12" 24" x 18"	W126B W162 W127			GR
645.4004			N.I.C.			W190-L			X
645.4003			N.I.C.	SEE M.U.T.C.D.		W190-R	SEE M.U.T.C.D.	SEE M.U.T.C.D.	X

NOTES:
1. LETTERS, NUMERALS, SYMBOLS AND BORDERS OR ANY PARTS OF THESE SHALL HEREAFTER BE REFERRED TO AS "CHARACTERS"
2. THE ALUMINUM SIGN POST OPTION, AS INDICATED ON STANDARD SHEETS 645-8R1, 645-9, 645-10, AND 645-11, WILL NOT BE ALLOWED ON THIS CONTRACT. ALL SIGN POSTS SHALL BE GALVANIZED STEEL.
3. TEXT NO. 1 TO BE PLACED 30" FROM EDGE OF PAVEMENT USING TYPE 14 POSTS.

LEGEND			
SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
W	WHITE OR SILVER	CAPS	CAPITAL LETTERS
G	GREEN	U.C.	UPPER CASE LETTERS
Y	YELLOW	L.C.	LOWER CASE LETTERS
B	BLACK	D	DEMOUNTABLE TYPE CHARACTERS
BL	BLUE	N.D.	NON DEMOUNTABLE CHARACTERS
R	RED		
REFL.	REFLECTORIZED		
NON-REFL.	NON REFLECTORIZED		
GR. MTD.	GROUND MOUNTED	O.H.	OVERHEAD MOUNTED
F	APPROX. LOCATION OF SIGN	C.S.M.	CANTILEVER MOUNTED SINGLE MAST ARM
(I)	LOCATION TEXT	C.D.M.	CANTILEVER MOUNTED DOUBLE MAST ARM
		C.C.M.	CANTILEVER CENTER MOUNTED
		F.S.	FRACTION SQUARE
		S.P.	SINGLE POST
		D.P.	DOUBLE POST

SIGN TEXT DATA
STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DRAWING No. S-1 SCALE NONE DATE 11/77 REGION 9



D95749

SIGN TEXT DATA SHEET

FED. RD. REG. NO.	STATE	FEDERAL PROJECT AID NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-88-1(62)	33	99

**INTERSTATE ROUTE 508
CENTRAL BRIDGE-SCHENECTADY COUNTY LINE
SCHOHARIE COUNTY**

LOCATION OF SIGNS			
TEXT NO.	LOCATION NO.	STATION	SIDE
35	41	RT 30 - 58 + 75	RT
17	42	RT 30 - 59 + 58	RT
28	43	RT 30 - 66 + 00	RT
32	44	RT 30 - 59 + 00	LT
18	45	RT 30 - 58 + 50	LT
34	47	RT 30 - 61 + 40	LT
31	48	RT 30 - 67 + 10	LT
30	50	RT 7 - 133 + 50	LT
35	51	RT 7 - 127 + 50	LT
38	52	RT 7 - 126 + 00	LT
29	53	RT 7 - 120 + 50	LT
30	55	RT 7 - 120 + 00	RT
37	56	RT 7 - 125 + 00	RT
33	57	RT 7 - 127 + 50	RT
29	58	RT 7 - 133 + 50	RT

TABLE OF SIGNS TO BE INSTALLED										TABLE OF SIGNS TO BE INSTALLED									
ITEM NO.	LOCATION NO.	TEXT NO.	TEXT	LETTER SIZE	APPROX. SIZE OF SIGN	M.U.T.C.D. NO.	COLOR		TYPE OF MOUNTING	ITEM NO.	LOCATION NO.	TEXT NO.	TEXT	LETTER SIZE	APPROX. SIZE OF SIGN	M.U.T.C.D. NO.	COLOR		TYPE OF MOUNTING
							BACKGROUND	CHARACTERS									BACKGROUND	CHARACTERS	
076461702	21,22,34,36,38	25	■	SEE M.U.T.C.D.	3' x 6"	W195-2	SEE M.U.T.C.D.	SEE M.U.T.C.D.	GR				NORTH 30 ↑	SEE M.U.T.C.D.	24' x 12"	M21	SEE	SEE	GR
076461701	12,19,20,29,37,39	26	■		3' x 6"	W195-1			GR	645.2849	41	36	WEST EAST 7 7 ← →		24' x 24"	M3-2	M.U.T.C.D.	M.U.T.C.D.	GR
645.2480			N.I.C. EAST 88		30' x 15" 36' x 36"	M39A M35-2			X						24' x 15"	M13-V			GR
645.2600	35,43	28	30		24' x 24"	M3-2			GR				EAST 7 ↑		24' x 12"	M19			GR
645.2600	53,58	29	7		24' x 24"	M3-2			GR	645.2849	56	37	NORTH SOUTH 30 30 ← →		24' x 15"	M13-V			GR
645.2620	50,55	30	JCT 30		24' x 15" 24' x 24"	M-9 M3-2			GR						24' x 12"	M21, M22			GR
645.2620	40,48	31	JCT 7		24' x 15" 24' x 24"	M-9 M3-2			GR	645.2740	52	38	WEST TO 7 88	SEE M.U.T.C.D.	24' x 12"	M20, M50	SEE M.U.T.C.D.	SEE M.U.T.C.D.	G
645.2620	44	32	SOUTH 30		24' x 12" 24' x 24"	M-22 M3-2			GR						24' x 24"	M3-2, M34-2			GR
645.2620	57	33	EAST 7		24' x 12" 24' x 24"	M-19 M3-2			GR										GR
645.2860	47	34	SOUTH TO 30 88 — EAST WEST 7 7 ← →		24' x 12" 24' x 24" 24' x 15" 24' x 12" 24' x 24" 24' x 15"	M-22, M50 M3-2, M34-2 M13-V, M13-H M19, M20 M3-2 M13-H			GR										GR
645.2860	51	35	WEST TO 7 88 ↑ SOUTH NORTH 30 30 ← →	SEE M.U.T.C.D.	24' x 12" 24' x 24" 24' x 15" 24' x 12" 24' x 24" 24' x 15"	M20, M50 M3-2, M34-2 M13-V M22, M21 M3-2 M13-H	SEE M.U.T.C.D.	SEE M.U.T.C.D.	GR										GR

NOTES:
 1. LETTERS, NUMERALS, SYMBOLS AND BORDERS OR ANY PARTS OF THESE SHALL HEREAFTER BE REFERRED TO AS "CHARACTERS"
 2. THE ALUMINUM SIGN POST OPTION, AS INDICATED IN STANDARD SHEETS, 645-8R1, 645-9, 645-10, AND 645-11 WILL NOT BE ALLOWED ON THIS CONTRACT. ALL SIGNS POSTS SHALL BE GALVANIZED STEEL.

LEGEND					
SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
W	WHITE OR SILVER	CAPS	CAPITAL LETTERS	GR. MTD.	GROUND MOUNTED
G	GREEN	U.C.	UPPER CASE LETTERS	O.H.	OVERHEAD MOUNTED
Y	YELLOW	L.C.	LOWER CASE LETTERS	C.S.M.	CANTILEVER MOUNTED SINGLE MAST ARM
B	BLACK	D	DEMOUNTABLE TYPE CHARACTERS	C.D.M.	CANTILEVER MOUNTED DOUBLE MAST ARM
BL.	BLUE	N.D.	NON DEMOUNTABLE CHARACTERS	C.C.M.	CANTILEVER CENTER MOUNTED
R	RED				
REFL.	REFLECTORIZED				
NON-REFL.	NON REFLECTORIZED				
				F	APPROX. LOCATION OF SIGN
				⊕	LOCATION TEXT
				F.S.	FRACTION SQUARE
				S.P.	SINGLE POST
				D.P.	DOUBLE POST

SIGN TEXT DATA			
STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION			
DRAWING No S-2	SCALE NONE	DATE 11/77	REGION 9

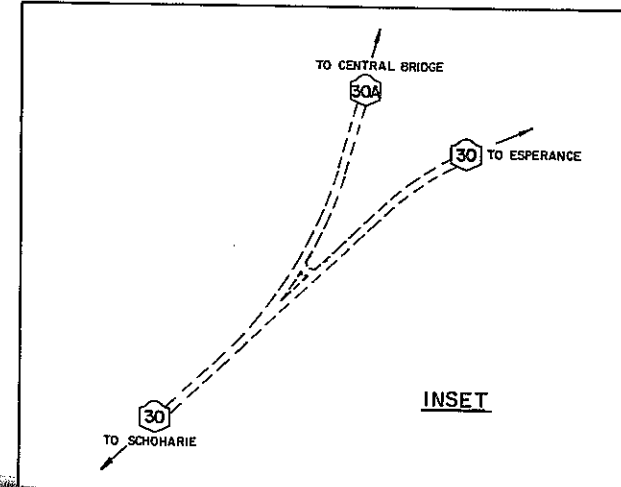
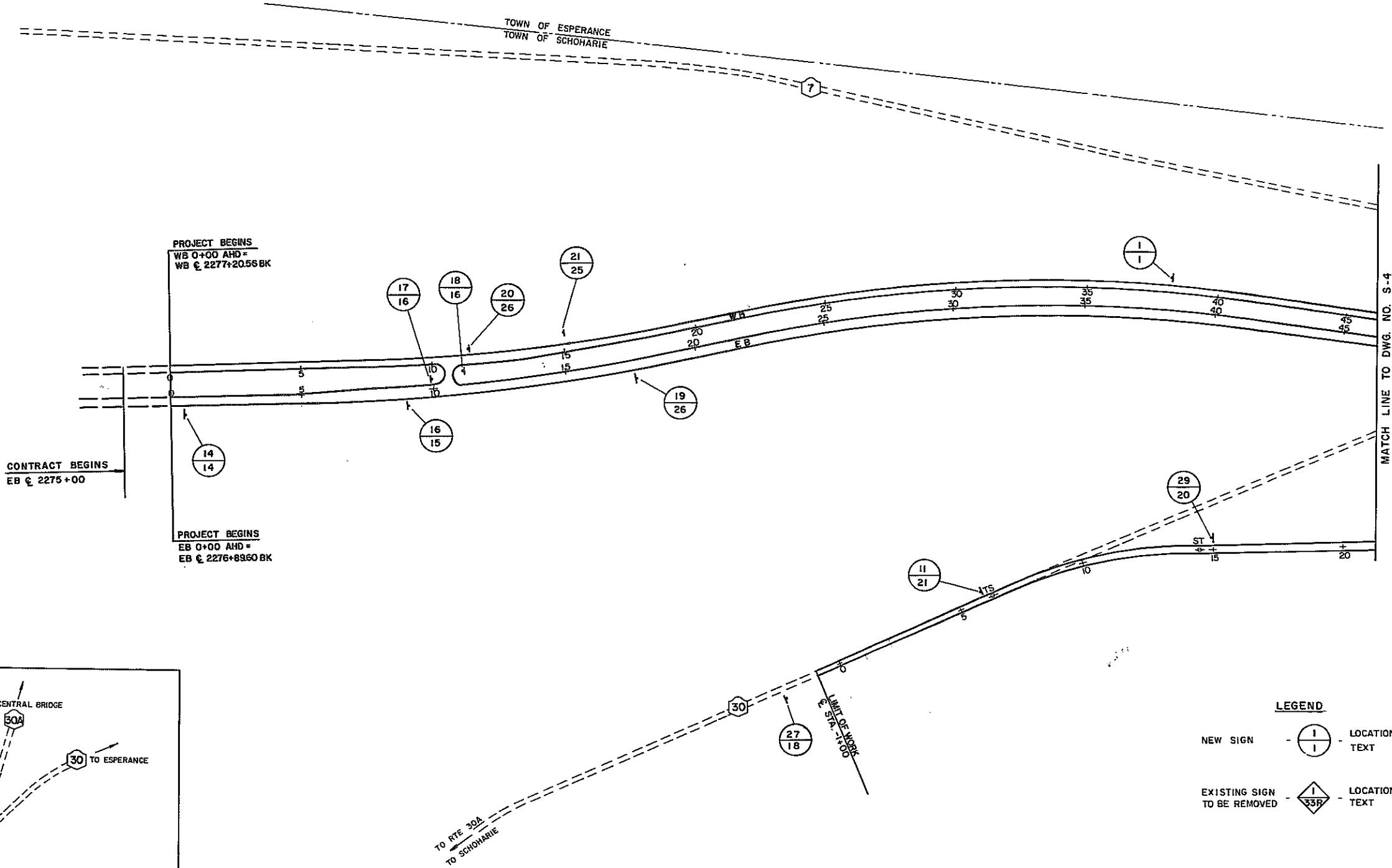
IN CHARGE OF _____
 DESIGNED BY _____
 CHECKED BY _____
 DATE _____
 REVIEWED BY _____
 DATE _____



D95749

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	I-98-1(52)	34	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE-SCHENECTADY COUNTY LINE				
SCHOHARIE COUNTY				

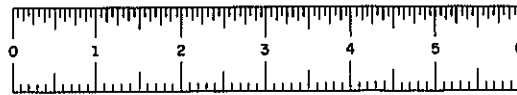
DATE _____ CHECKED BY _____ DRAFTED BY _____ CHECKED BY _____ ESTIMATED BY _____ CHECKED BY _____ DESIGNED BY _____ IN CHARGE OF _____



LEGEND

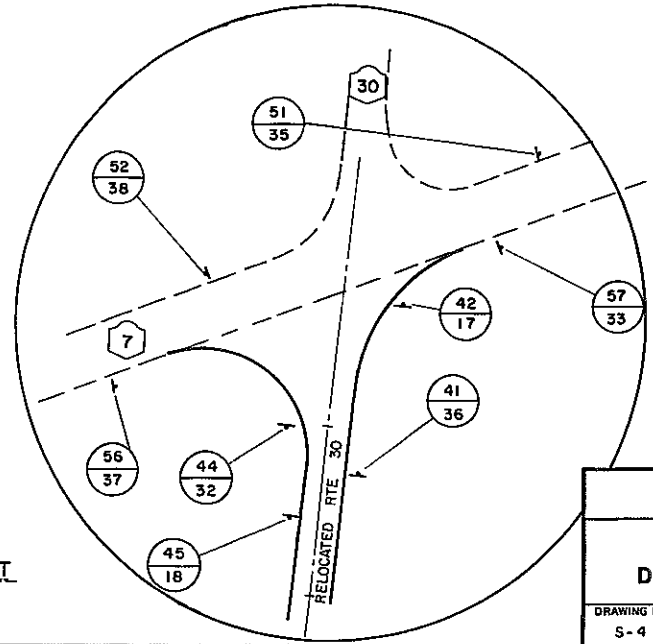
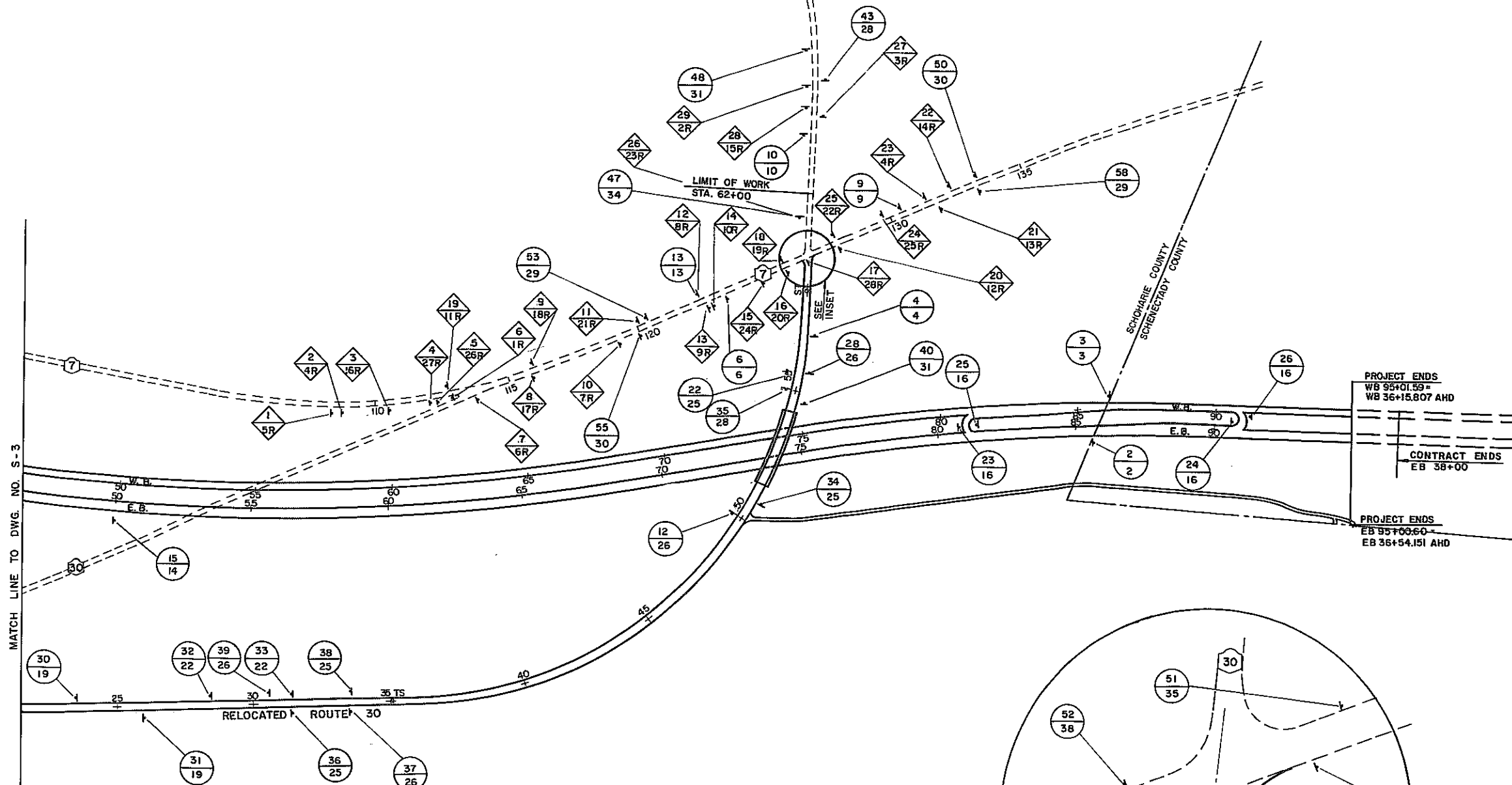
NEW SIGN	(1/1)	LOCATION TEXT
EXISTING SIGN TO BE REMOVED	(1/33R)	LOCATION TEXT

HW'Y. SIGN LOCATION PLAN			
STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION			
DRAWING No. S-3	SCALE 1" = 200'	DATE 1/77	REGION 9



D95749

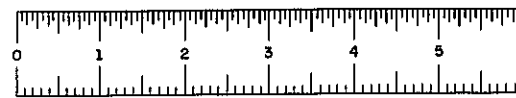
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	I-88-1(52)	35	99
INTERSTATE ROUTE 508				
CENTRAL BRIDGE-SCHENECTADY COUNTY LINE				
SCHOHARIE COUNTY				



HW'Y. SIGN LOCATION PLAN			
STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION			
DRAWING No. S-4	SCALE 1" = 200'	DATE 11/77	REGION 9

DESIGNED BY _____
 CHECKED BY _____
 ESTIMATED BY _____
 CHECKED BY _____
 DRAFTED BY _____
 CHECKED BY _____
 DATE _____

HC 47-2 (5/76)
IN CHARGE OF _____



D95749

SIGN TEXT DATA SHEET FOR REMOVALS

FED. RD. REG. NO.	STATE	FEDERAL PROJECT AID NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-8-1162	36	99

INTERSTATE ROUTE 508
CENTRAL BRIDGE-SCHENECTADY COUNTY LINE
SCHOHARIE COUNTY

LOCATION OF SIGNS TO BE REMOVED

TEXT NO.	LOCATION NO.	STATION	SIDE
5R	1	RT 7 - 108 + 40	RT
4R	2	RT 7 - 108 + 80	RT
16R	3	RT 7 - 110 + 50	RT
27R	4	RT 7 - 112 + 00	RT
26R	5	RT 7 - 112 + 30	RT
1R	6	RT 7 - 113 + 00	RT
6R	7	RT 7 - 113 + 60	RT
17R	8	RT 7 - 115 + 95	RT
18R	9	RT 7 - 116 + 00	LT
7R	10	RT 7 - 119 + 35	RT
21R	11	RT 7 - 120 + 25	LT
8R	12	RT 7 - 122 + 50	LT
9R	13	RT 7 - 122 + 50	RT
10R	14	RT 7 - 122 + 95	RT
24R	15	RT 7 - 124 + 90	RT
20R	16	RT 7 - 125 + 85	RT
26R	17	RT 7 - 126 + 60	RT
19R	18	RT 7 - 125 + 85	LT
11R	19	RT 7 - 112 + 80	LT
12R	20	RT 7 - 127 + 95	RT
13R	21	RT 7 - 121 + 85	RT
14R	22	RT 7 - 122 + 50	LT
4R	22	RT 7 - 131 + 60	LT
25R	24	RT 7 - 129 + 80	LT
22R	25	RT 7 - 127 + 90	LT
23R	26	RT 30 - 61 + 20	LT
3R	27	RT 30 - 64 + 25	RT
15R	28	RT 30 - 65 + 00	LT
2R	29	RT 30 - 65 + 90	LT

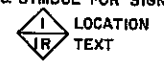
TABLE OF SIGNS TO BE REMOVED

ITEM NO.	LOCATION NO.	TEXT NO.	TEXT	LETTER SIZE	APPROX. SIZE OF SIGN	M.U.T.C.D. NO.	COLOR		TYPE OF MOUNTING
							BACKGROUND	CHARACTERS	
647.04	6	1R			9.0 S.F.				
647.04	29	2R			6.5 S.F.				
647.04	27	3R			4.0 S.F.				
647.04	2,23	4R			6.5 S.F.				
647.04	1	5R			6.25 S.F.				
647.04	7	6R			5.8 S.F.				
647.04	10	7R			6.25 S.F.				
647.04	12	8R	TOWN OF SCHOHARIE		9.0 S.F.				
647.04	13	9R	TOWN OF ESPERANCE		9.32 S.F.				
647.04	14	10R			8.5 S.F.				
647.04	19	11R			6.0 S.F.				
647.04	20	12R			6.0 S.F.				
647.04	21	13R			4.0 S.F.				

TABLE OF SIGNS TO BE REMOVED

ITEM NO.	LOCATION NO.	TEXT NO.	TEXT	LETTER SIZE	APPROX. SIZE OF SIGN	M.U.T.C.D. NO.	COLOR		TYPE OF MOUNTING
							BACKGROUND	CHARACTERS	
647.04	22	14R			6.25 S.F.				
647.04	28	15R			8.5 S.F.				
647.05	3	16R	DUANESBURG 8		14.0 S.F.				
647.05	8	17R			12.0 S.F.				
647.05	9	18R			17.0 S.F.				
647.05	18	19R			12.0 S.F.				
647.05	16	20R			17.0 S.F.				
647.05	11	21R			12.5 S.F.				

NOTES:
1. LETTERS, NUMERALS, SYMBOLS AND BORDERS OR ANY PARTS OF THESE SHALL HEREAFTER BE REFERRED TO AS "CHARACTERS"
2. ALL SIGNS TO BE REMOVED ARE GROUND MOUNTED.
3. SYMBOL FOR SIGNS TO BE REMOVED:



SIGN REMOVAL DATA SHEET
STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION

DATE	SCALE	DRAWING NO.	REGION NO.
11/77	NONE	S-5	9

DESIGNED BY _____ CHECKED BY _____ REVIEWED BY _____ DATE _____



D95749

SIGN TEXT DATA SHEET FOR REMOVALS

FED. NO. REG. NO.	STATE	FEDERAL PROJECT	AID NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-88-1(52)		37	99

INTERSTATE ROUTE 508
CENTRAL BRIDGE-SCHENECTADY COUNTY LINE
SCHOHARIE COUNTY

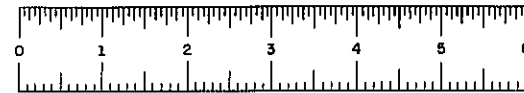
TABLE OF SIGNS TO BE REMOVED										TABLE OF SIGNS TO BE REMOVED													
ITEM NO.	LOCATION NO.	TEXT NO.	TEXT	LETTER SIZE	APPROX. SIZE OF SIGN	M.U.T.C.D. NO.	C O L O R		TYPE OF MOUNTING		ITEM NO.	LOCATION NO.	TEXT NO.	TEXT	LETTER SIZE	APPROX. SIZE OF SIGN	M.U.T.C.D. NO.	C O L O R		TYPE OF MOUNTING			
							BACKGROUND	CHARACTERS										BACKGROUND	CHARACTERS				
647.06	25	22R	WEST 7 ↓ SOUTH NORTH 30 30 ↑ ↓		26.0 S.F.																		
647.06	26	23R	SOUTH 30 → EAST WEST 7 7 ← →		26.0 S.F.					647.07	17	28R	← DUANESBURG 8 COBLESKILL 11 → ← →		41.25 S.F.								
647.06	15	24R	↑ DUANESBURG 8 ← ESPERANCE 5		33.0 S.F.																		
647.06	24	25R	↑ COBLESKILL 11 ← ESPERANCE 5		33.0 S.F.																		
647.06	5	26R	SCHOHARIE 5 COBLESKILL 11		21.0 S.F.																		
647.06	4	27R	EAST 7 ↓ NORTH SOUTH 30 30 ↑ ↓		26.0 S.F.																		

DATE REVIEWED BY DATE CHECKED BY DATE DESIGNED BY IN CHARGE OF

NOTES:
 1. LETTERS, NUMERALS, SYMBOLS AND BORDERS OR ANY PARTS OF THESE SHALL HEREAFTER BE REFERRED TO AS "CHARACTERS"
 2. ALL SIGNS TO BE REMOVED ARE GROUND MOUNTED.
 3. SYMBOL FOR SIGNS TO BE REMOVED:
 ⬇ LOCATION
 IR TEXT

FOR LISTING SEE
Dwg. No. S-5 THE PREVIOUS SHEET.

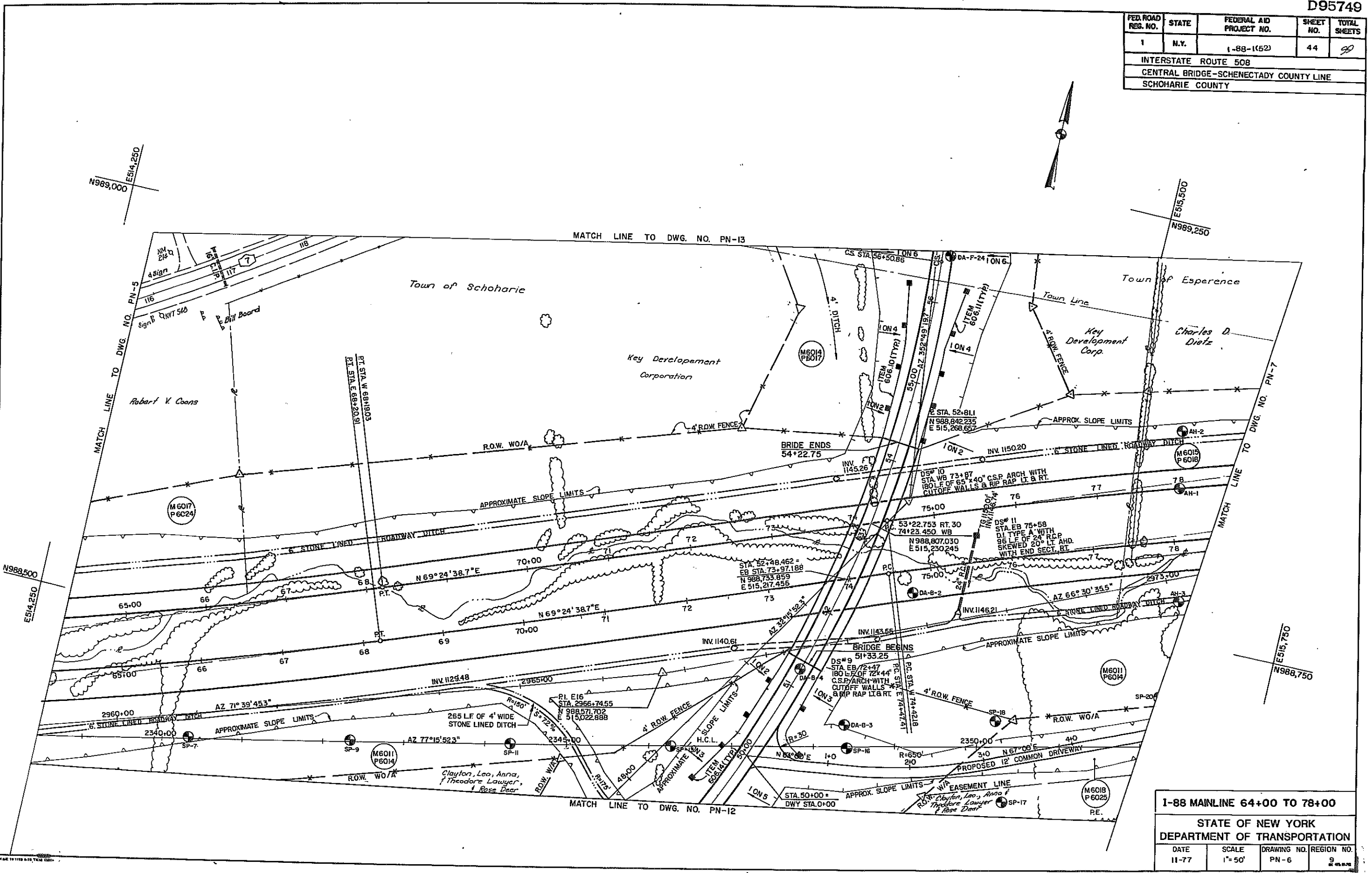
SIGN REMOVAL DATA SHEET			
STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION			
DATE 11/77	SCALE NONE	DRAWING NO. S-6	REGION NO. 9



D95749

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	1-88-1(52)	44	9
INTERSTATE ROUTE 50B				
CENTRAL BRIDGE-SCHENECTADY COUNTY LINE				
SCHOHARIE COUNTY				

IN CHARGE OF _____ DESIGNED BY _____ CHECKED BY _____ REVIEWED BY _____ DATED _____

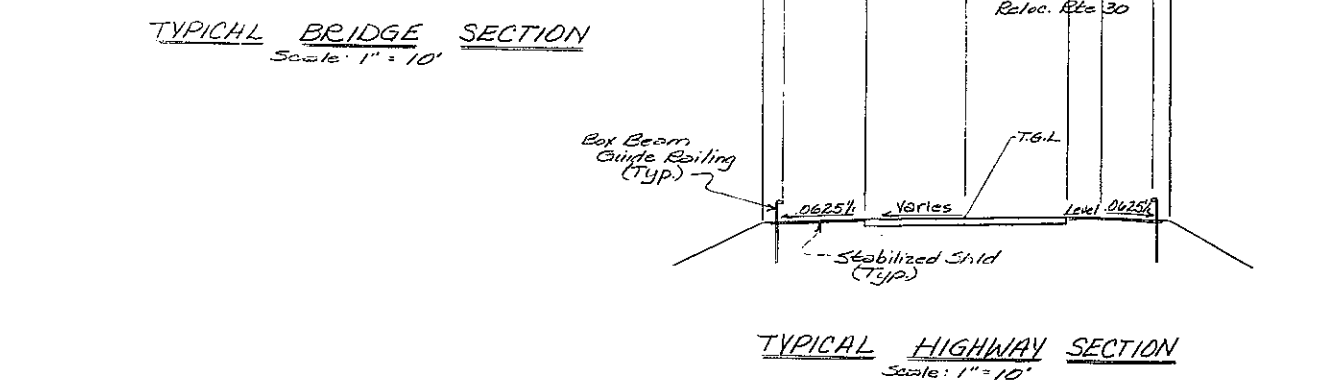
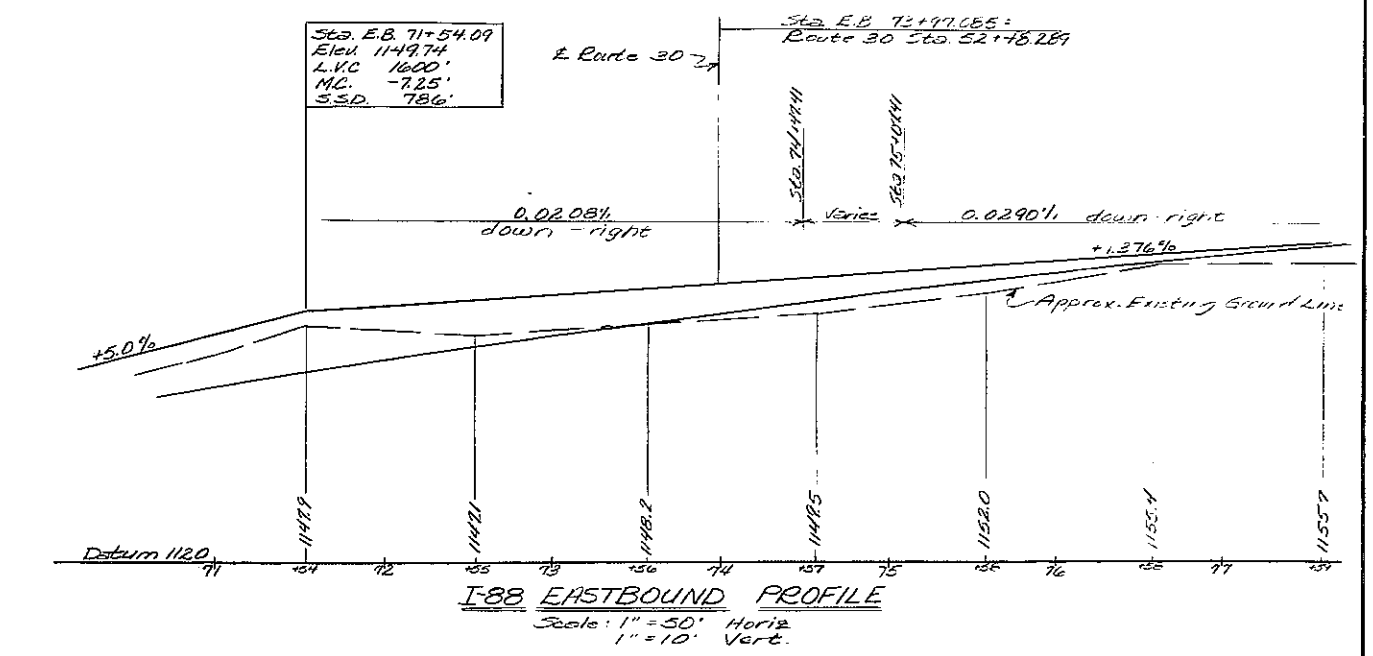
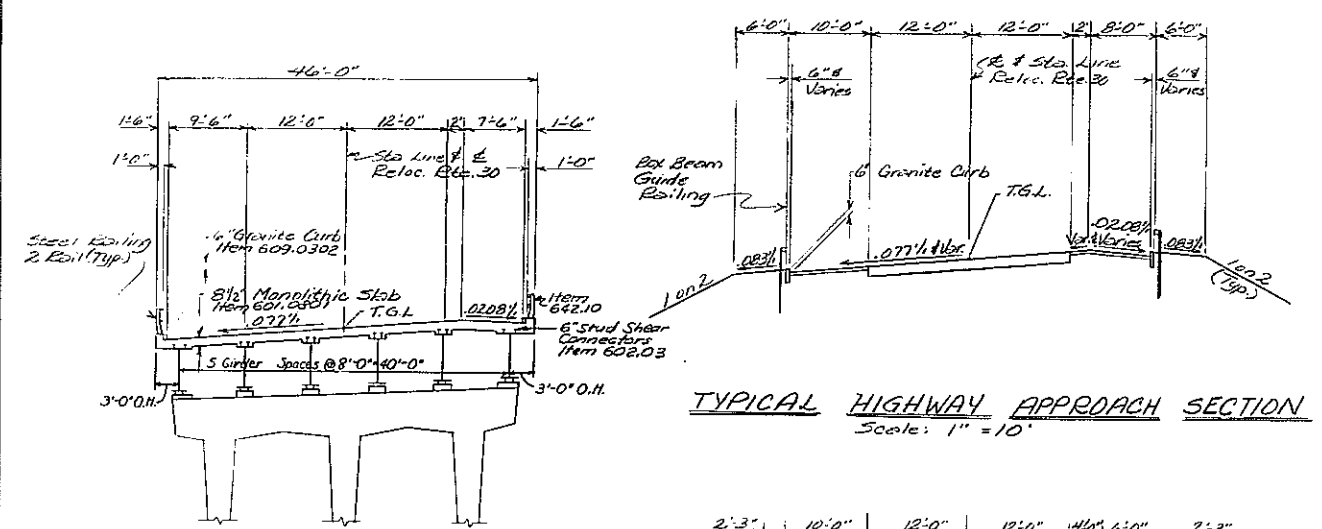
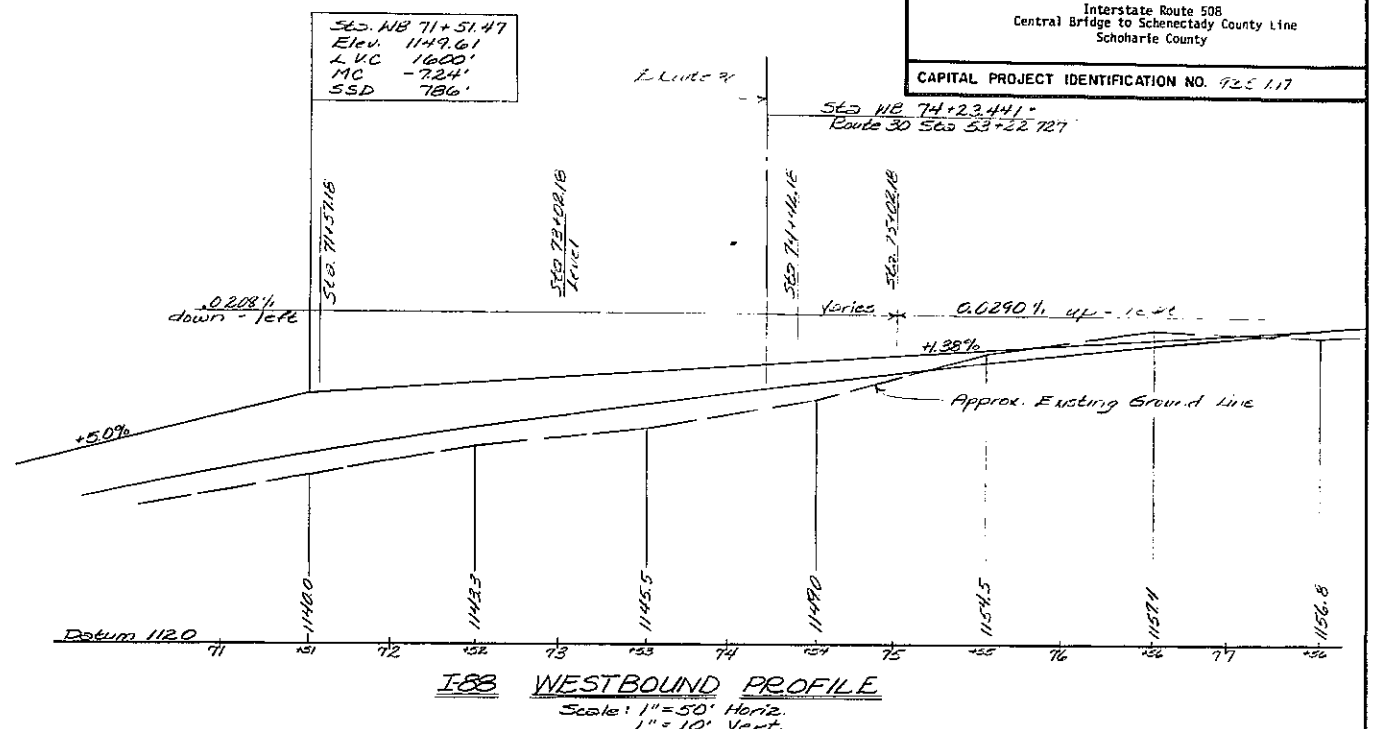
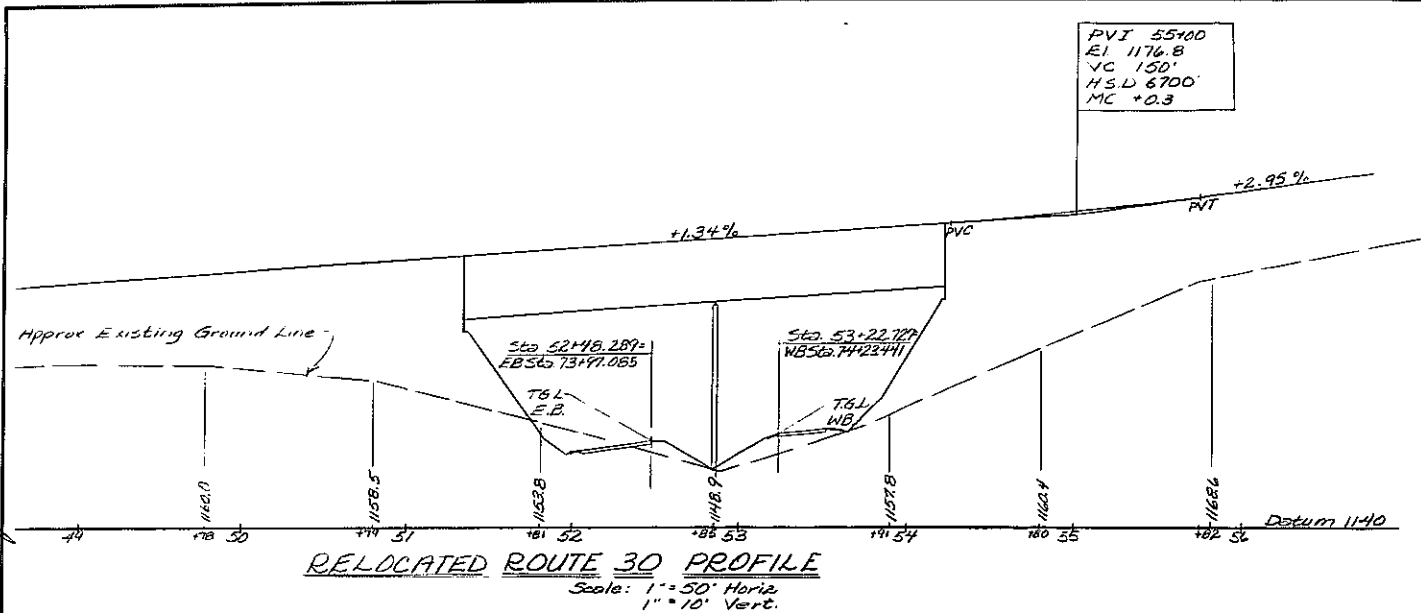


1-88 MAINLINE 64+00 TO 78+00			
STATE OF NEW YORK			
DEPARTMENT OF TRANSPORTATION			
DATE	SCALE	DRAWING NO.	REGION NO.
11-77	1" = 50'	PN-6	9

FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-1(62)	75	99

Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County

CAPITAL PROJECT IDENTIFICATION NO. 925 117



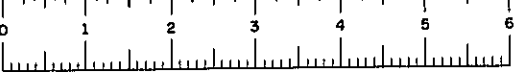
STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I-88

SECTIONS AND PROFILES

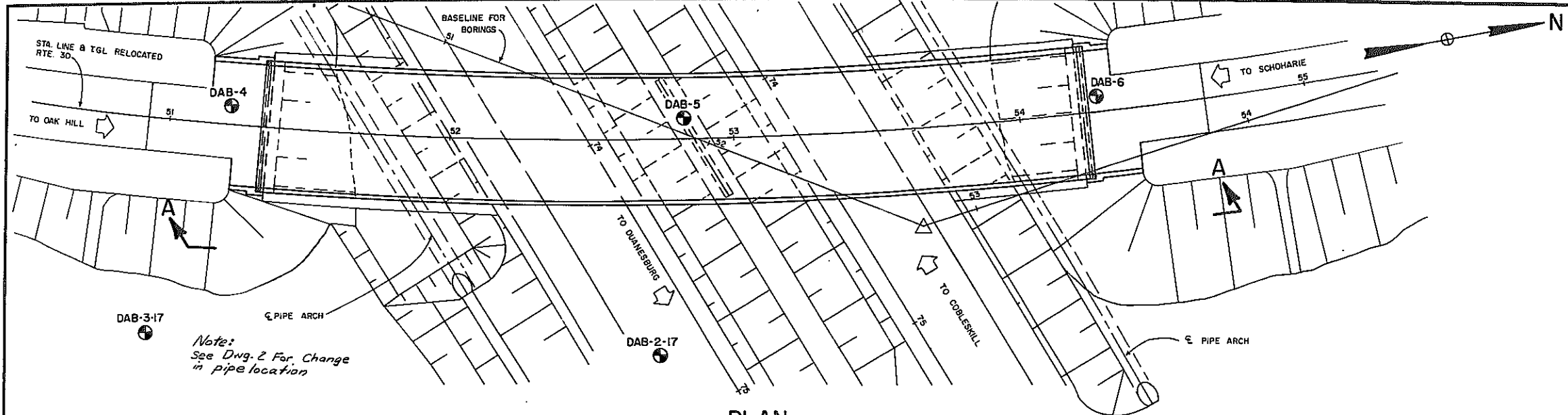
PROJ. ENG.	DATE MADE 9/77
SQUAD	DRAWING NO. 3 OF 27

DESIGNED BY: [redacted] CHECKED BY: [redacted] REVISIONS: [redacted]

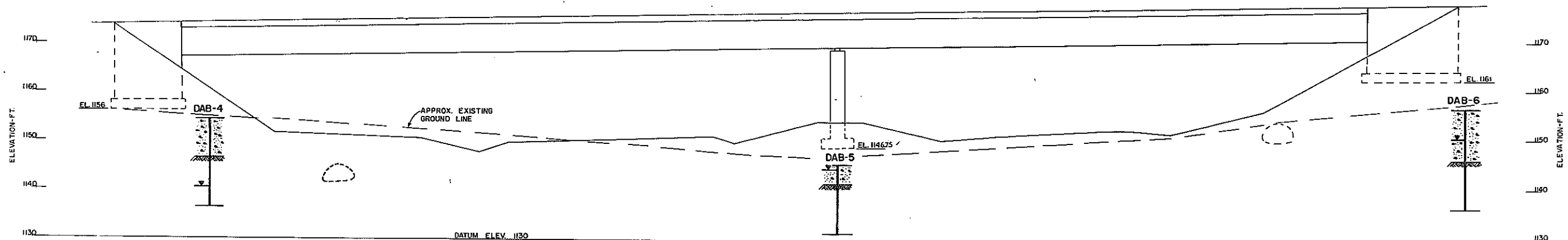


D95749

FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	I-88-1(52)	76	99
Interstate Route 508 Central Bridge to Schoharie County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NUMBER: 9357.17				



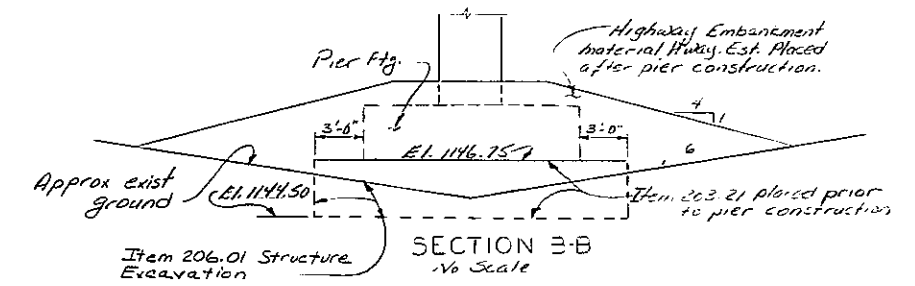
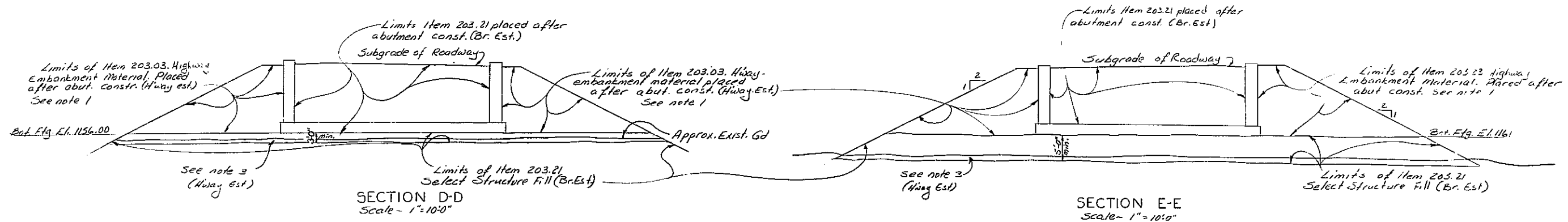
PLAN
SCALE 1"=20'



SECTION A-A
BORINGS PROJECTED TO SECTIONLINE PARALLEL TO CENTERLINE OF PIER
SCALE 1"=10'

<p>REFERENCE PLANS</p> <p>Preliminary Structure Plans Used for Analysis were</p> <p>Prepared By: The Structures Design and Construction Subdivision.</p> <p>Scale: 1"=20' Date: 9/14/76</p> <p>Prepared By: [Redacted]</p> <p>Drawn By: [Redacted]</p> <p>Dwg. Review: [Redacted]</p> <p>Checked By: [Redacted]</p>	<p>GENERAL NOTES</p> <p>The subsurface explorations shown hereon were made between 1/11/77 to 1/28/77 by the Regional Soils Section.</p> <p>1) General soil and rock (where encountered) strata descriptions and indicated boundaries are based on an engineering interpretation of all available subsurface information by the Soil Mechanics Bureau and may not necessarily reflect the actual variation in subsurface conditions encountered in individual borings and samples. Detailed data and field interpretations of conditions encountered in individual borings are shown on the subsurface exploration logs.</p> <p>2) The observed water levels and/or conditions indicated on the subsurface profiles are as recorded at the time of exploration. These water levels and/or conditions may vary considerably, with time, according to the prevailing climate, rainfall or other factors and are otherwise dependent on the duration of and methods used in the explorations program.</p> <p>3) Sound engineering judgment was exercised in preparing the subsurface information presented hereon. This information was prepared and is intended for State design and estimate purposes only. Its presentation on the plans or elsewhere is for the purpose of providing intended users with access to the same information available to the State. This subsurface information interpretation is presented in good faith and is not intended as a substitute for personal investigation, independent interpretations or judgment of the Contractor.</p> <p>4) All structure details shown hereon are for illustrative purposes only and may not be indicative of the final design conditions shown in the contract plans.</p> <p>5) Footing elevations shown are as indicated at the time of this drawing's preparation.</p>	<p>LEGEND</p> <p>The following tables summarize the descriptive information used on this profile.</p> <table border="0" style="width: 100%;"> <tr> <td colspan="2">Density (Non Plastic Soils)</td> </tr> <tr> <td>Very Loose</td> <td>0-3</td> </tr> <tr> <td>Loose</td> <td>4-8</td> </tr> <tr> <td>Medium Compact</td> <td>9-20</td> </tr> <tr> <td>Compact</td> <td>21-35</td> </tr> <tr> <td>Very Compact</td> <td>over 35</td> </tr> <tr> <td colspan="2">Consistency (Plastic Soils)</td> </tr> <tr> <td>Very Soft</td> <td>0-2</td> </tr> <tr> <td>Soft</td> <td>3-6</td> </tr> <tr> <td>Firm</td> <td>7-12</td> </tr> <tr> <td>Stiff</td> <td>13-20</td> </tr> <tr> <td>Hard</td> <td>over 20</td> </tr> </table> <p>The system for describing soil materials shown on this drawing is detailed in "Soil Description Procedure" Official Issuance No. 7.41-5 STP 2/75 prepared by the New York State Department of Transportation Soil Mechanics Bureau.</p>	Density (Non Plastic Soils)		Very Loose	0-3	Loose	4-8	Medium Compact	9-20	Compact	21-35	Very Compact	over 35	Consistency (Plastic Soils)		Very Soft	0-2	Soft	3-6	Firm	7-12	Stiff	13-20	Hard	over 20	<p>SYMBOLS</p> <p>DRILL HOLE</p> <p>OBSERVED WATER LEVEL</p> <p>Compact to Very Compact Brown Sandy Silt, Grovelly</p> <p>LEDGEROCK</p> <p style="text-align: right;">BORINGS DAB-2-17 AND DAB-3-17 ARE SHOWN IN PLAN BUT NOT IN PROFILE DUE TO THE DISTANCE FROM THE STRUCTURE.</p>	<p>APPROVED <i>JUNE 6 1977</i></p> <p>[Redacted Signature]</p> <p>DIRECTOR SOIL MECHANICS BUREAU</p> <p>REGION NO. 9</p> <p>COUNTY SCHOHARIE</p> <p>DWG. NO. 9 SM 1865</p>	<p>STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION DESIGN AND CONSTRUCTION DIVISION</p> <p>SUBSURFACE PLAN</p> <p>GENERAL SUBSURFACE PROFILE FOR RELOCATED ROUTE 30 OVER INTERSTATE ROUTE 508 (I-88)</p> <p style="text-align: right;">DRAWING NO. 4 OF 27</p>
Density (Non Plastic Soils)																													
Very Loose	0-3																												
Loose	4-8																												
Medium Compact	9-20																												
Compact	21-35																												
Very Compact	over 35																												
Consistency (Plastic Soils)																													
Very Soft	0-2																												
Soft	3-6																												
Firm	7-12																												
Stiff	13-20																												
Hard	over 20																												

FED. RD. REG. NO.	STATE	FEDERAL AD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1088-1152	78	99
Interstate Route 508 Central Bridge to Schenectady County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				



NOTES:

1. Highway Embankment Material placed within these limits shall have maximum dimension of six inches (6"). Quantity to be included in Highway Estimate.
2. Highway Embankment Material and Item 203.21, shall be placed simultaneously, in contact, on both sides of the vertical payment line.
3. Topsoil shall be stripped beneath abutments and piers in fills less than 20' in height from a rectangular or trapezoidal area bounded by lines 15 feet outside the abutment and wingwall footings, or to the toe of slope whichever is less. The depth of stripping shall be determined by the Regional Soils Engineer and displayed on the Highway Cross Sections by the Regional Design Engineer.

ABUTMENTS AND PIER

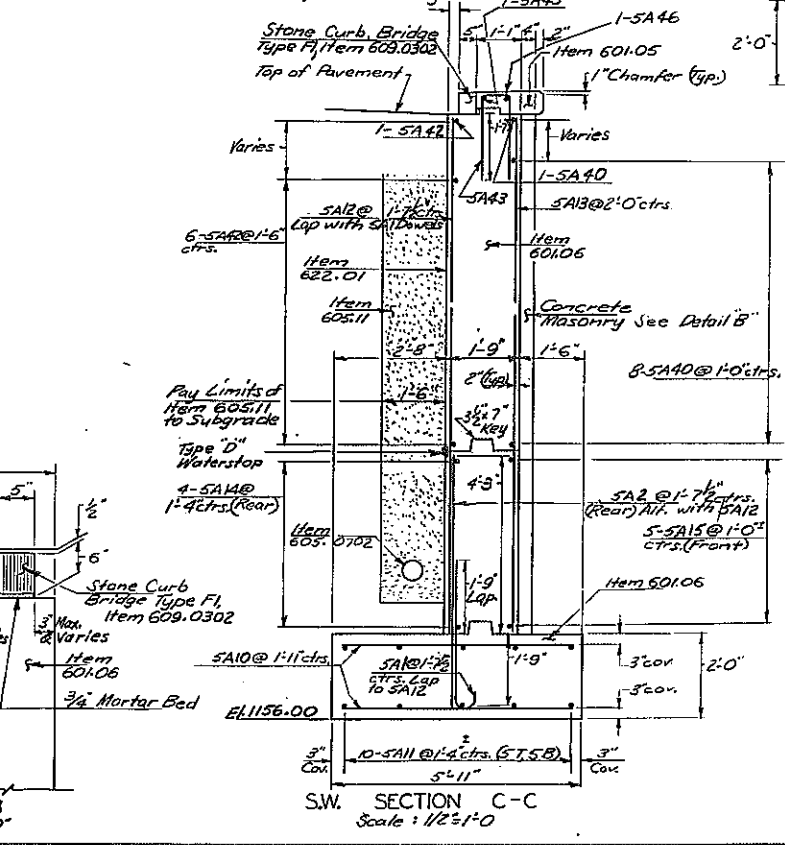
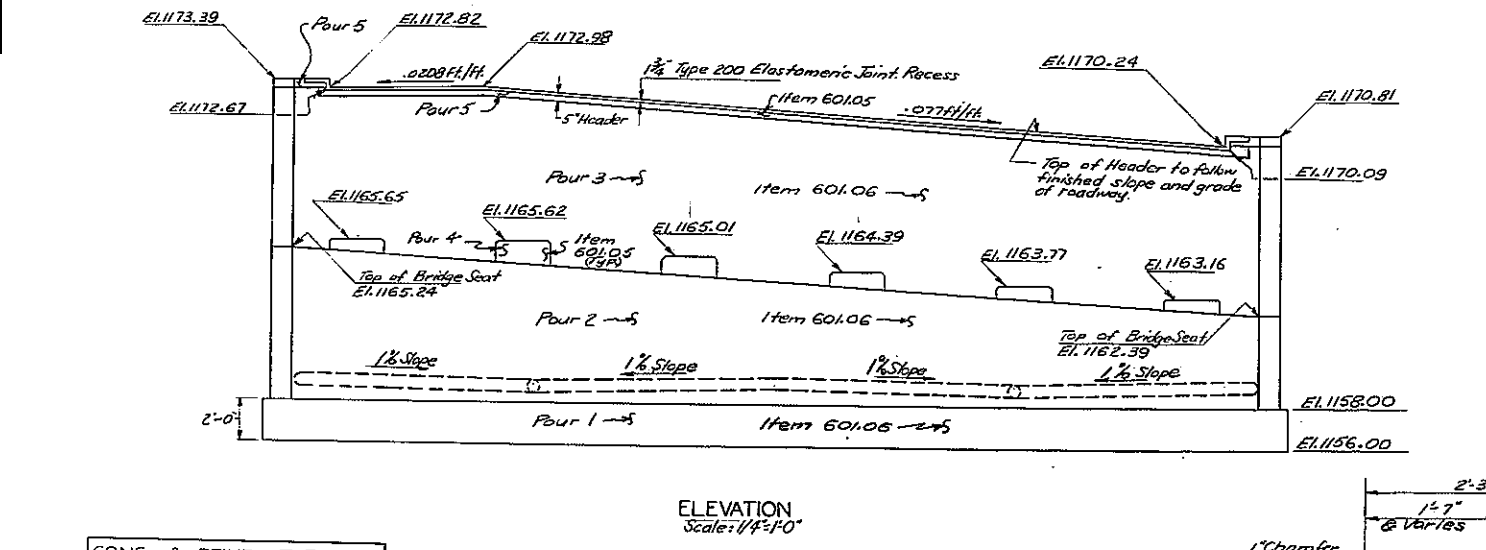
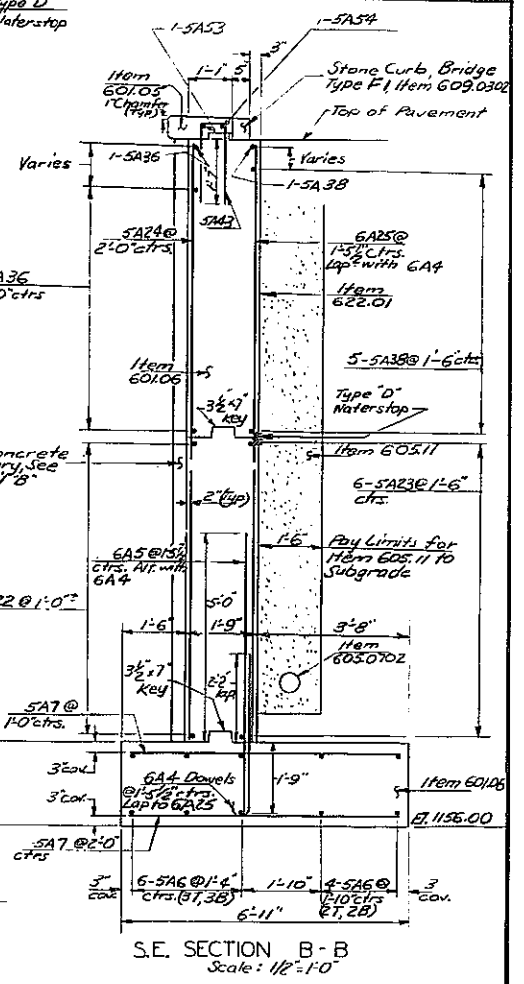
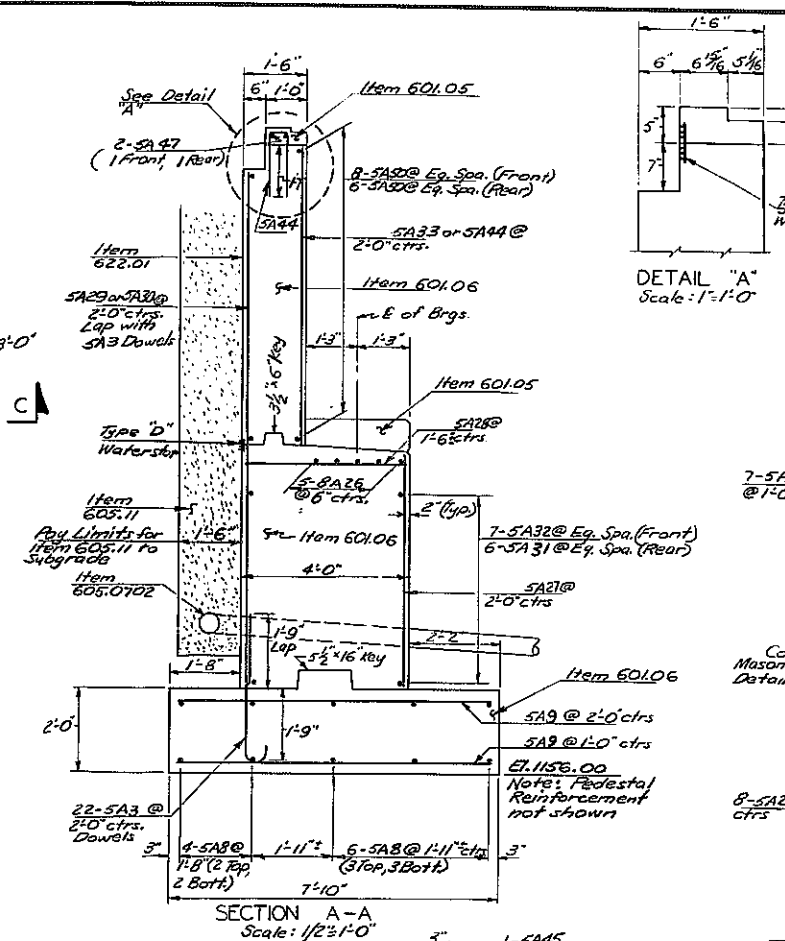
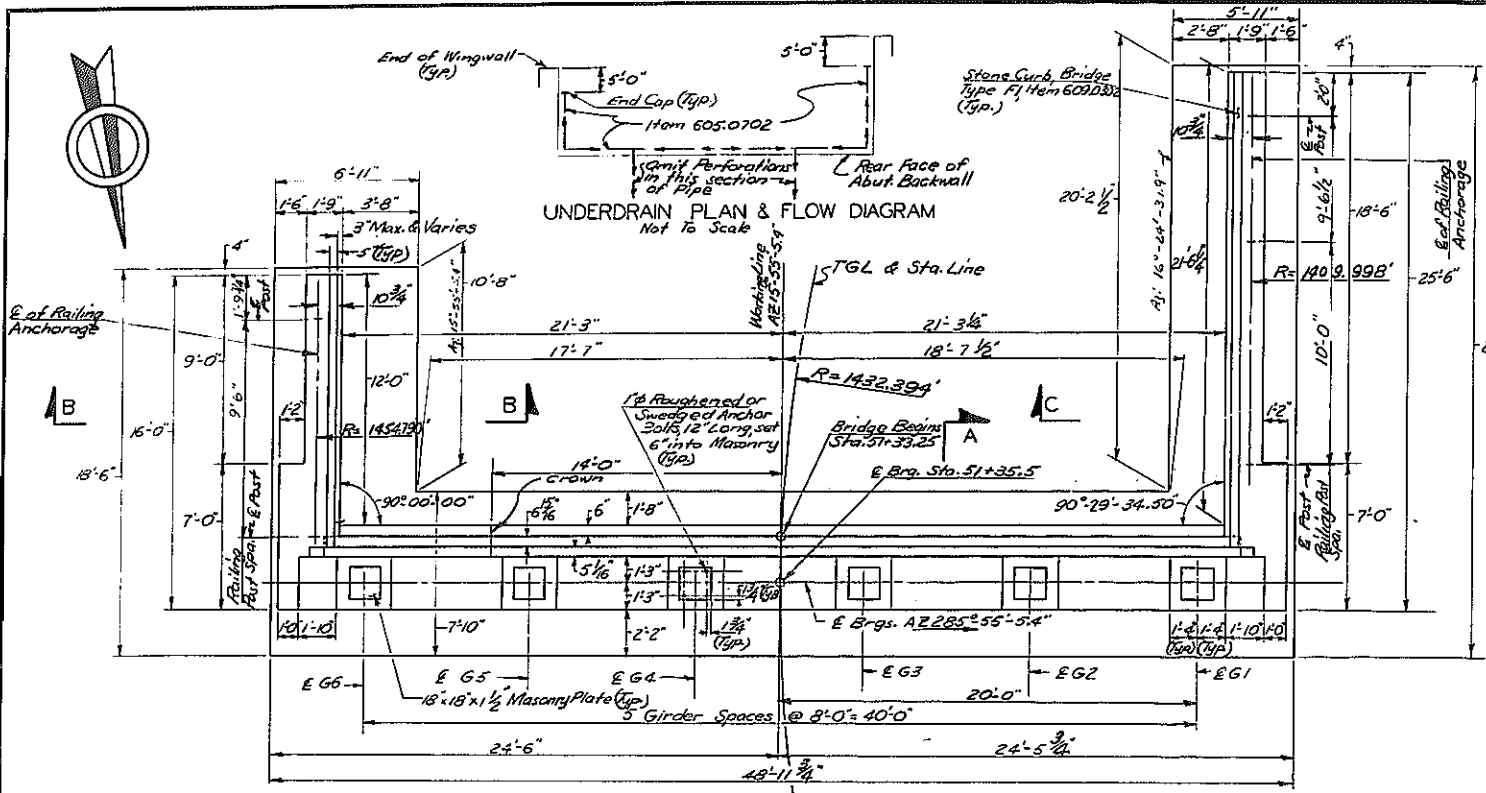
The abutments and pier are to be supported on spread footings placed on compacted Select Structure Fill, Item 203.21. These spread footings are designed for a maximum design bearing pressure of 2.5 tons per square foot.

At the Pier, the existing ground shall be removed to an Elevation of 1144.50. The lateral limits of this removal shall be three feet (3') outside the pier footing edges. Backfill material shall be compacted Select Structure Fill, Item No. 203.21.

STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION DIVISION OF CONSTRUCTION	
Relocated Route 50 over I - 88	
EMBANKMENT SECTION	
PROJ. EN	DATE MADE 9/27
SQUAD	DRAWING NO. 6 OF 27

DESIGNED BY: [REDACTED] CHECKED BY: [REDACTED] DETAILED BY: [REDACTED] CHECKED BY: [REDACTED]

FED. RD. REG. NO.	STATE	FEDERAL AD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-1(62)	79	99
Interstate Route 508 Central Bridge to Schoharie County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				



Note: 1. For Joint Details see Dwg. No. 17618 of 26.
2. For Keyway & Waterstop Details see Dwg. 22 of 27.
3. For Design Purposes the Foundation Pressure does not exceed 2.5 Tons Per Square Foot.

ITEM NO.	CONC & REINF. TABLE	601.05	601.06	602.0201
Fig. Pour 1	—	CY	42.76	29 28 #
Stem Pour 2	—	CY	49.84	3201 #
Bkwl. Pour 3	—	CY	41.00	1994 #
Headwall	1.74	CY	—	91 #
PEDS. 4	1.22	CY	—	94 #
TOTALS	2.96	CY	133.60	7408 #

Note: For Pay Limits of Split faced Concrete Masonry, Item 617.02 & Concrete Block Masonry, Item 09617.9999, see Dwg. No. 22 of 27.

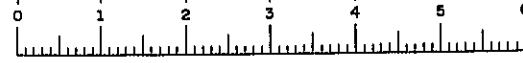
STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I-88

SOUTH ABUTMENT PLAN & SECTION

PROJ. ENG. SQUAD 12	DATE MADE 9/77
DRAWING NO. 7 OF 27	

DESIGNED BY: [Redacted] CHECKED BY: [Redacted] DETAILED BY: [Redacted]

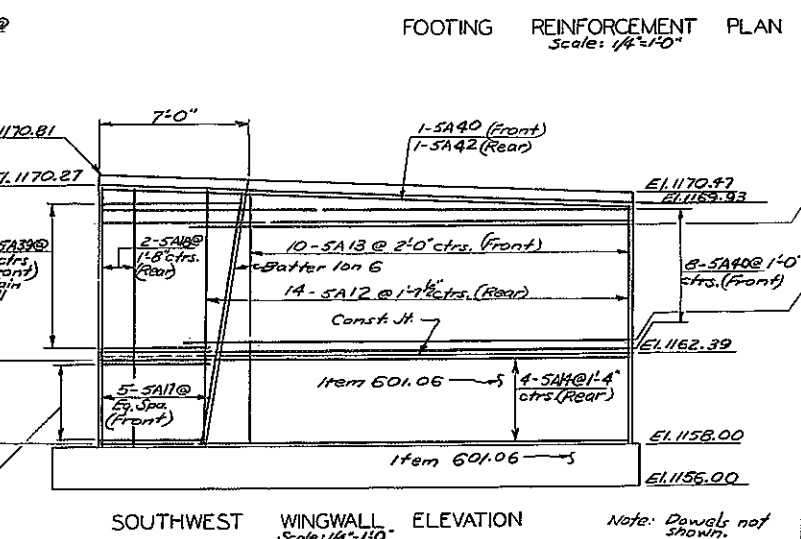
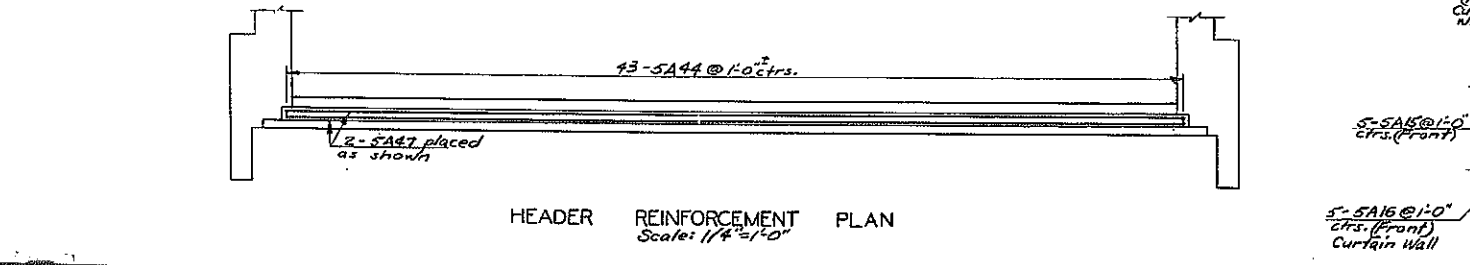
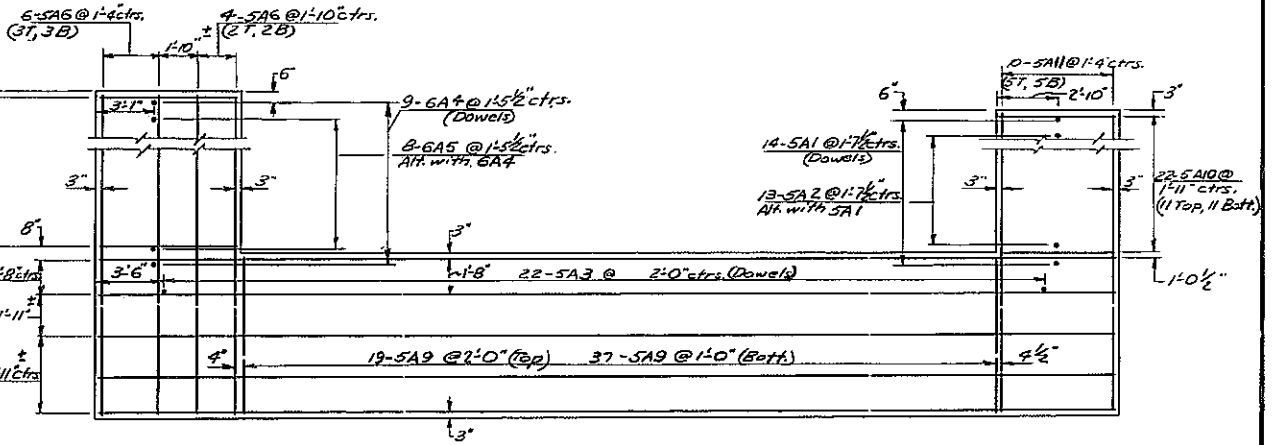
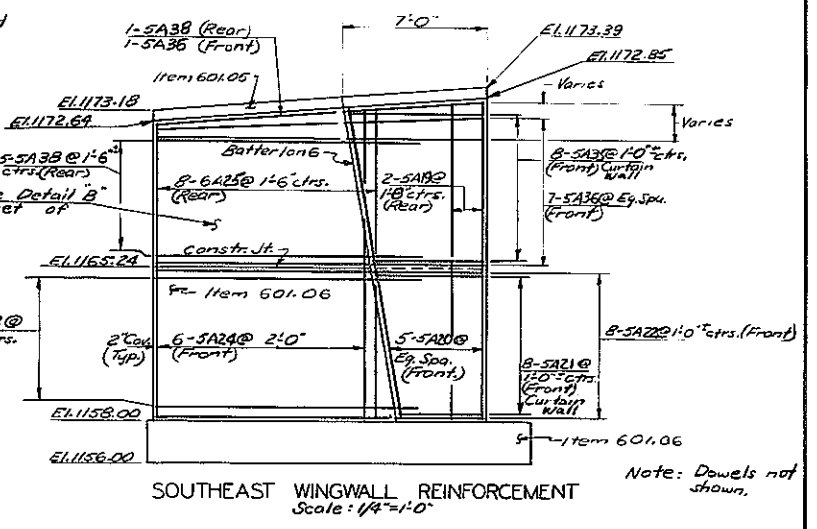
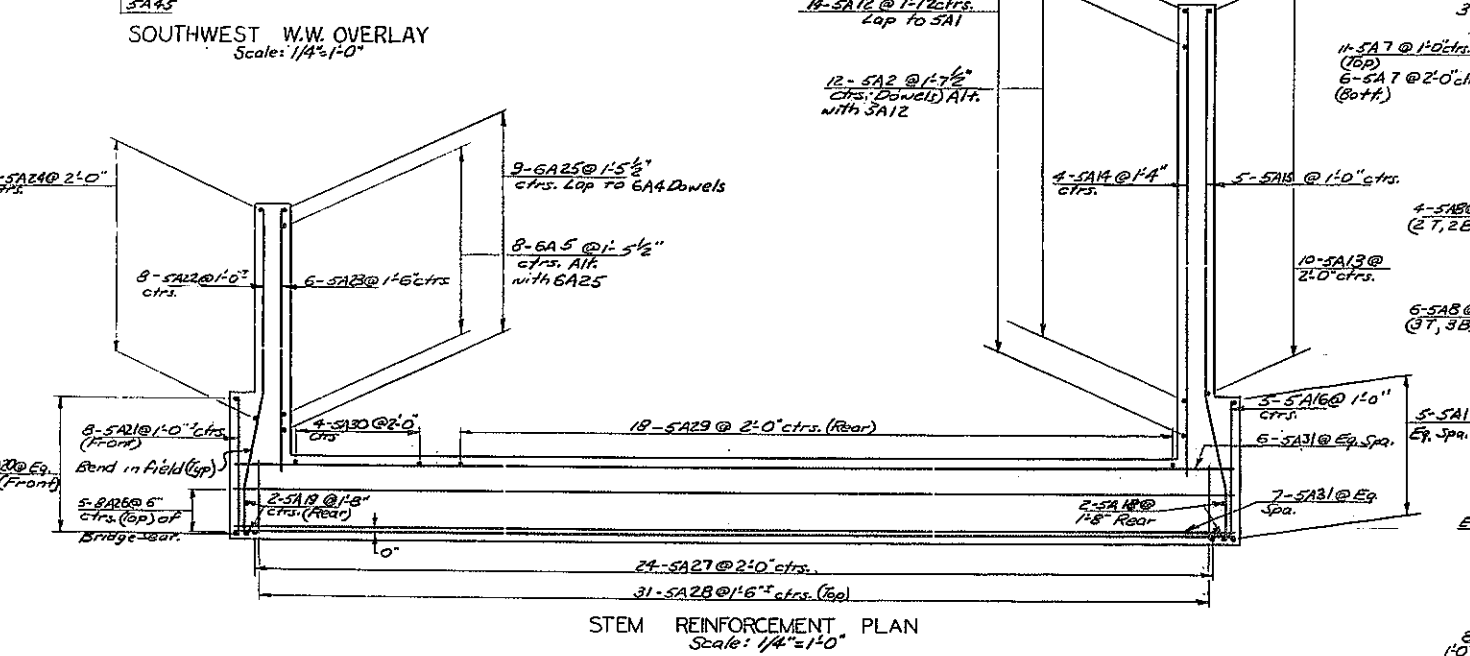
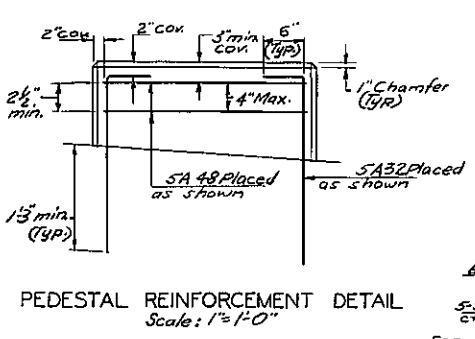
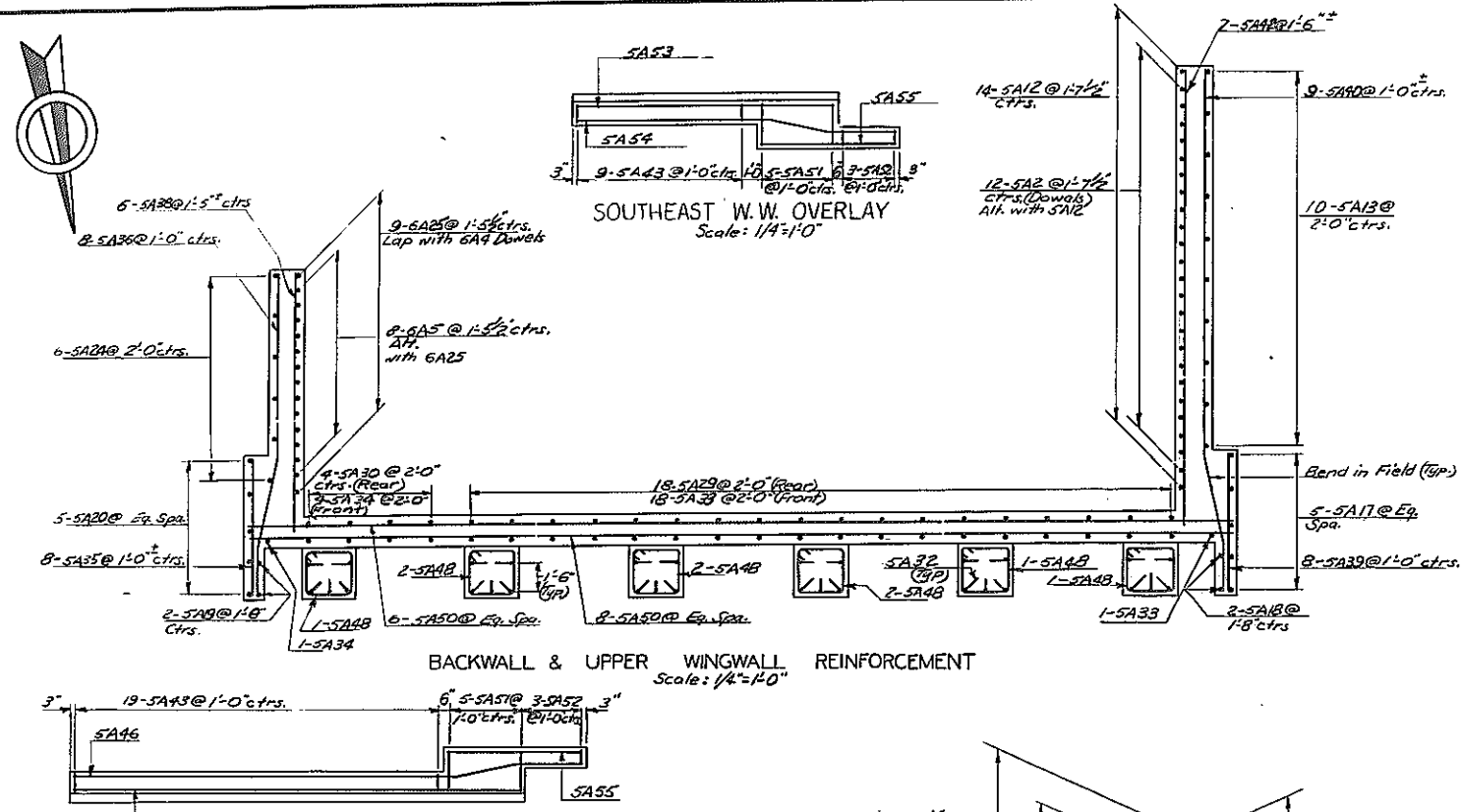


D95749

FED. RD. REG. NO.	STATE	FEDERAL AD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-38-1(52)	80	99

Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County

CAPITAL PROJECT IDENTIFICATION NO. 935717



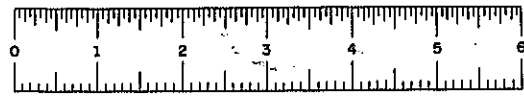
STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I - 88

SOUTH ABUTMENT REINFORCEMENT

PROJ. ENGR.	DATE MADE
SQUAD	9/77
	DRAWING NO. 8 OF 27

DESIGNED BY: [redacted] CHECKED BY: [redacted] REVISIONS: [redacted]

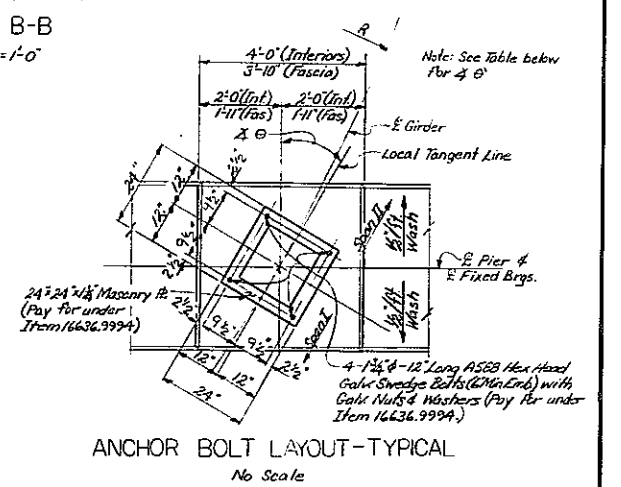
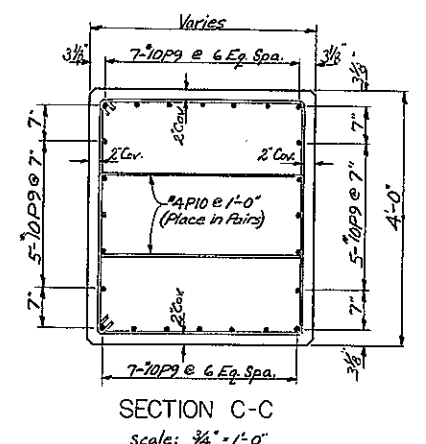
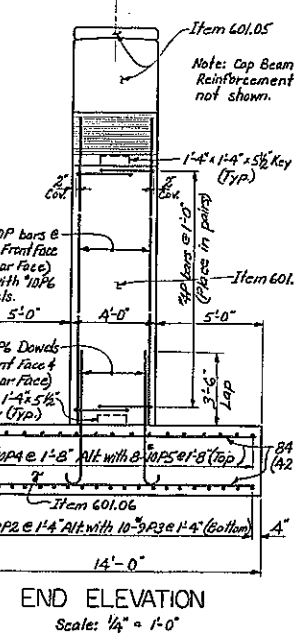
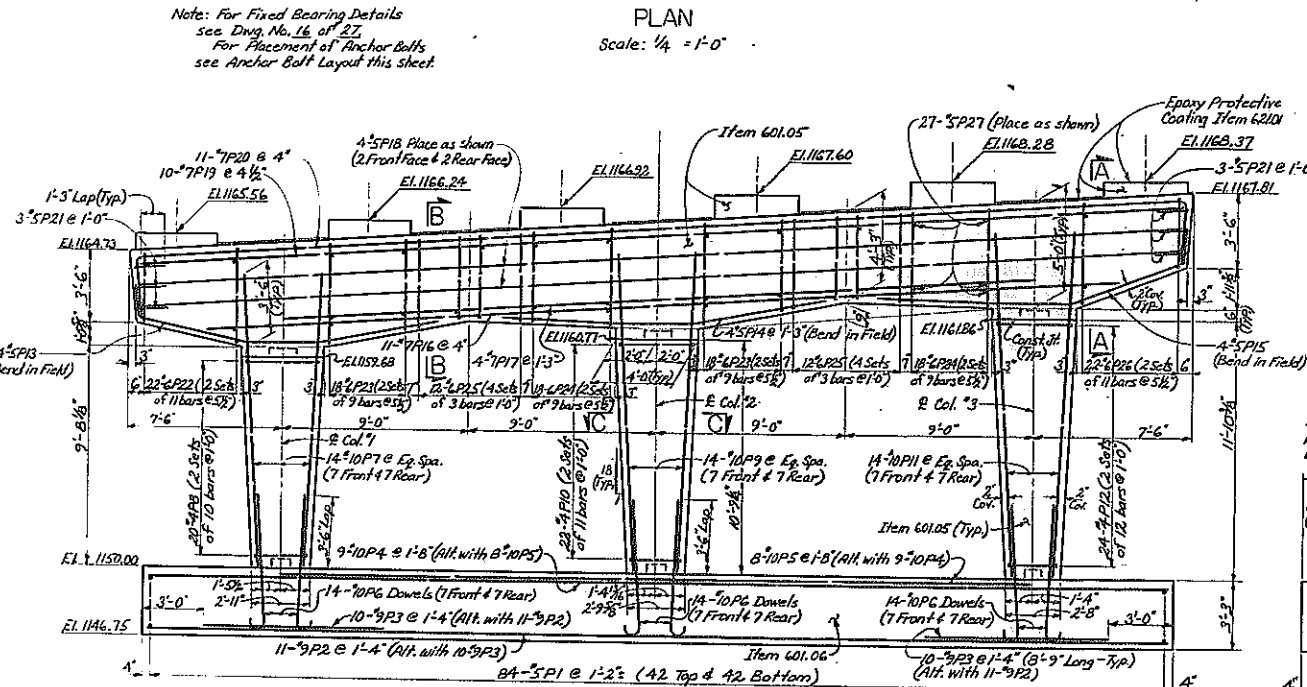
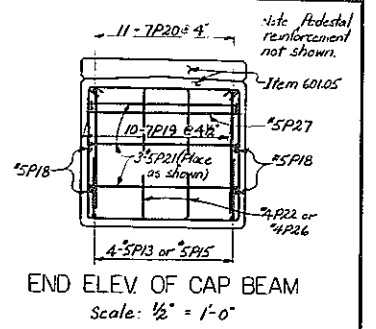
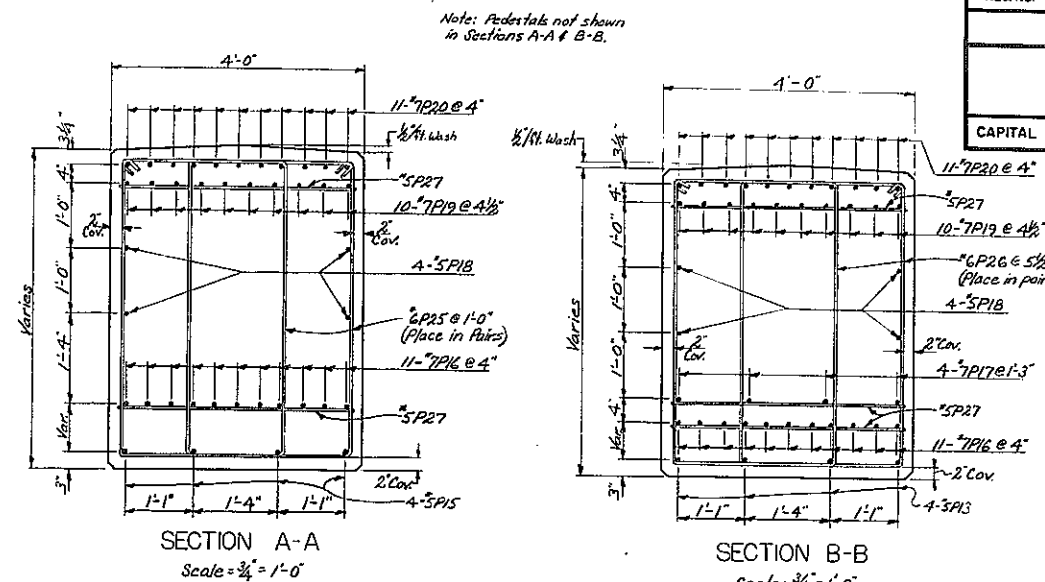
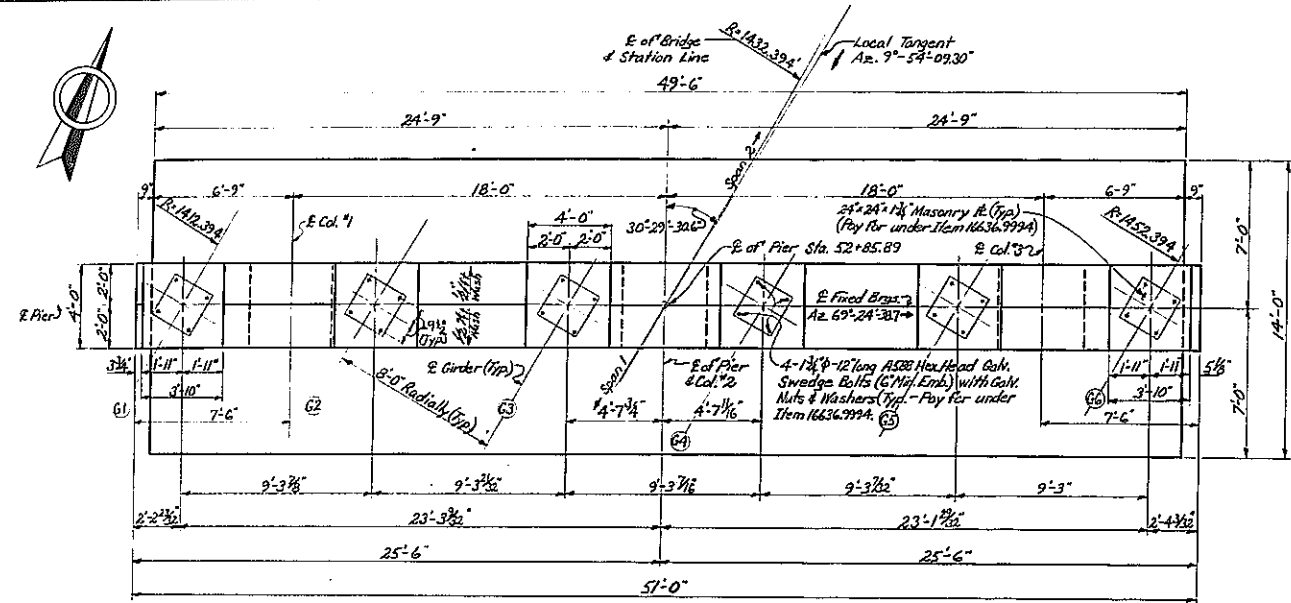


D95749

FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
NEW YORK	§ 88-1(62)	81	99	

Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County

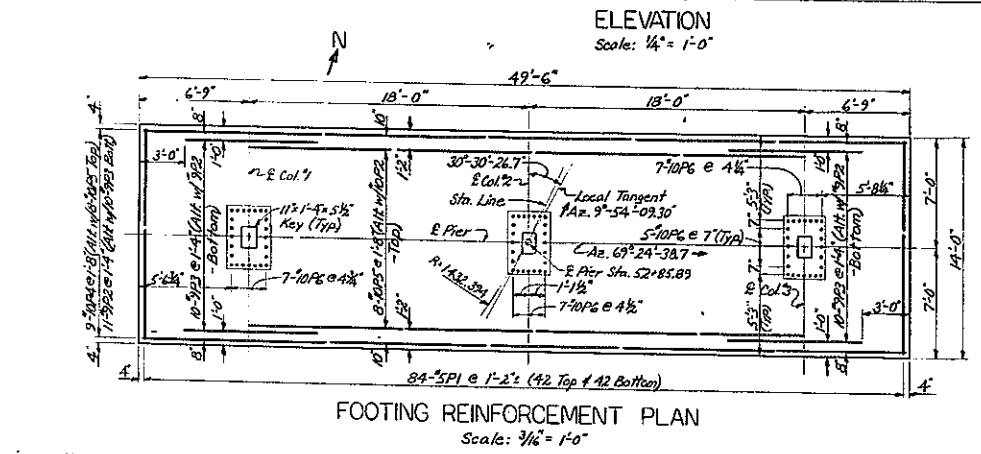
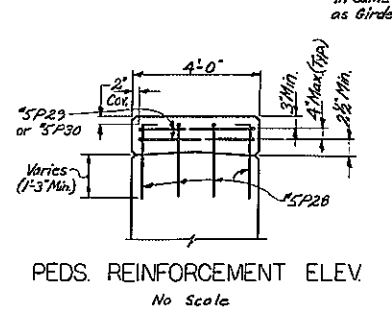
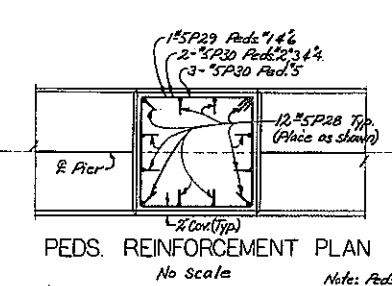
CAPITAL PROJECT IDENTIFICATION NO. 935717



GIRDER NO.	CORRESPONDING X @
G1	30'-58" - 14.7'
G2	30'-46" - 38.2'
G3	30'-35" - 10.9'
G4	30'-29" - 52.5'
G5	30'-12" - 42.9'
G6	30'-01" - 47.0'

Notes: Footing Reinforcement cover shall be 3" & Columns, Cap Beam, & Pedestal Reinforcement shall be 2" cover unless indicated otherwise. All exposed edges of concrete shall be chamfered 1" unless indicated otherwise. For Design Purposes, the foundation pressure does not exceed 2.5 Tons/ft². For Keyway Details see Dwg. No. 22 of 27.

POUR	TABLE OF QUANTITIES		
	Item 601.05	Item 601.06	Item 602.0201
Footings		83.44 C.Y.	9201 #
Column I	4.99 C.Y.		1563 #
Column II	5.46 C.Y.		1690 #
Column III	5.90 C.Y.		1817 #
Cap Beam	35.96 C.Y.		7619 #
Pedestals	2.83 C.Y.		179 #
TOTALS	55.14 C.Y.	83.44 C.Y.	22,069 #



STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I - 88

PIER

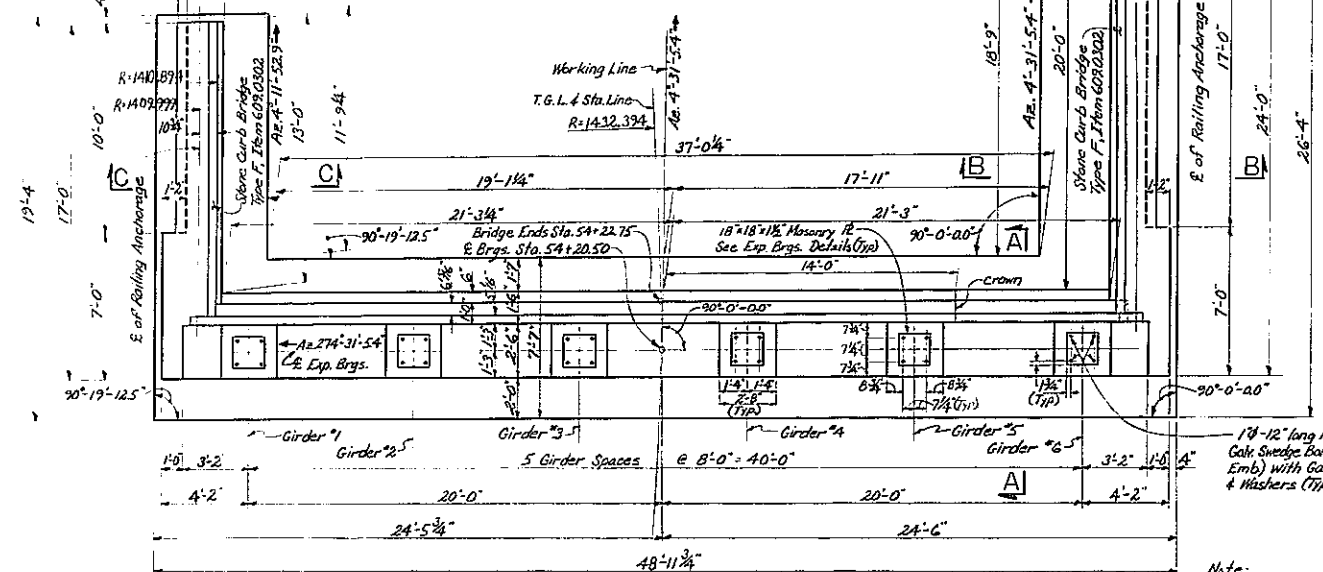
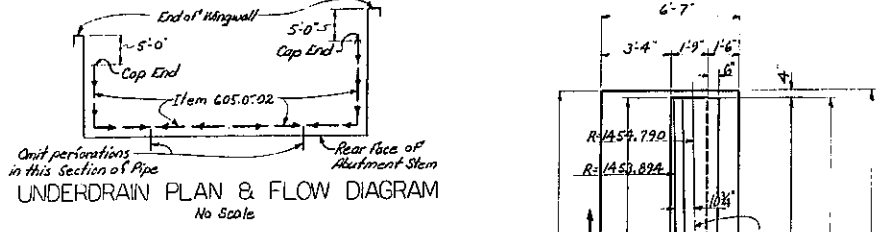
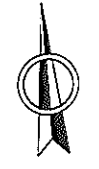
PROJ. ENG. [Redacted] DATE MADE 9/77
SQUAD 2 DRAWING NO. 9 OF 27

FED. RD. REG. NO.	STATE	FEDERAL AD. PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-1(52)	82	99

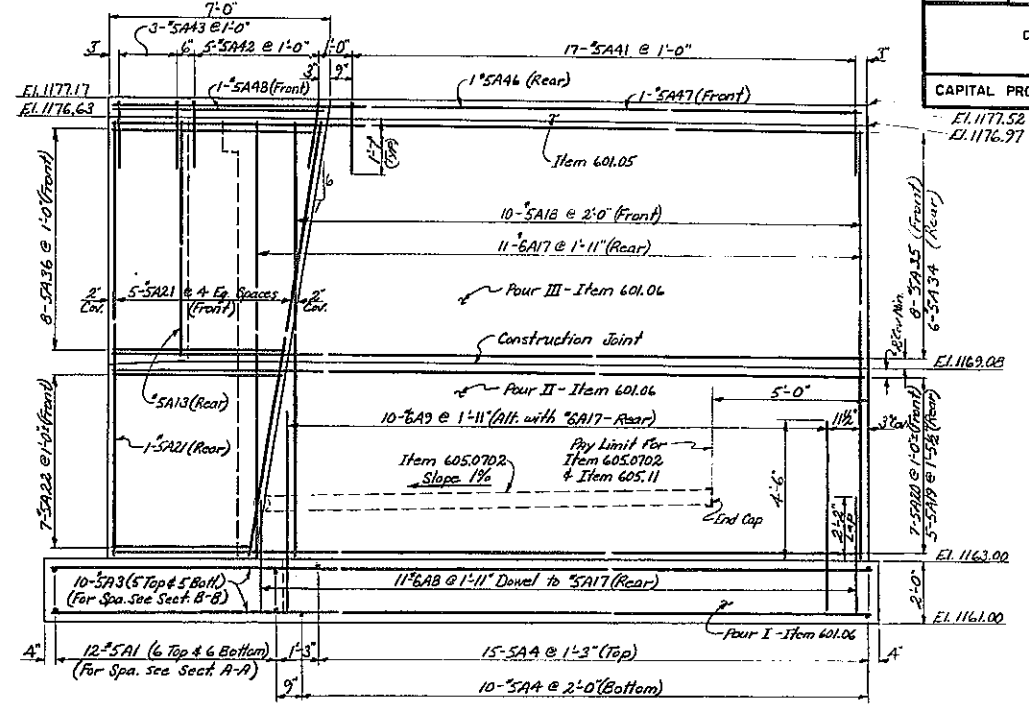
Interstate Route 508
Central Bridge to Schenectady County Line
Schoharie County

CAPITAL PROJECT IDENTIFICATION NO. 935717

Note:
All walls parallel to their corresponding footing.



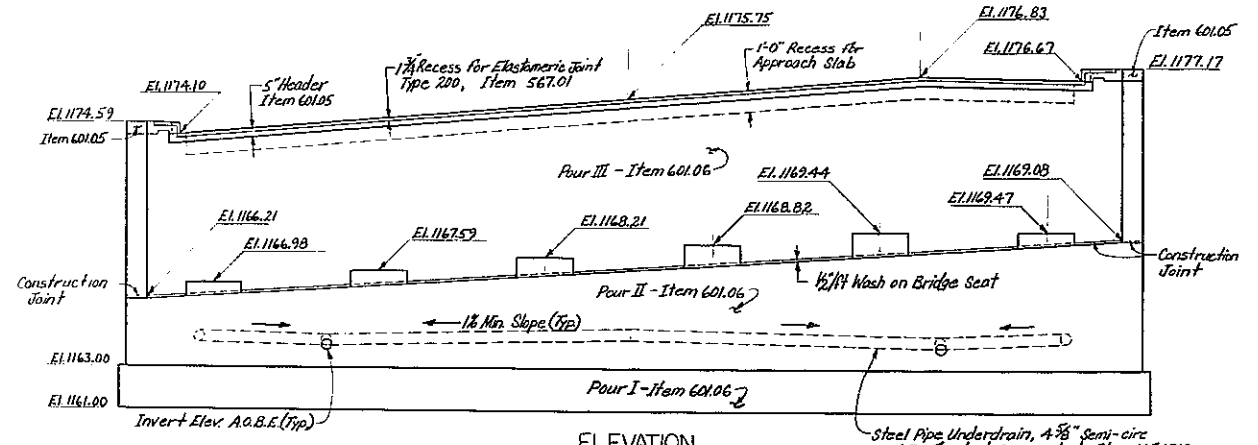
NORTH ABUT. - PLAN
Scale: 1/4" = 1'-0"



NORTHEAST WINGWALL ELEVATION
Scale: 3/8" = 1'-0"

Note:
For Sections A-A, B-B, and C-C see Dwg. No. 11 of 27.

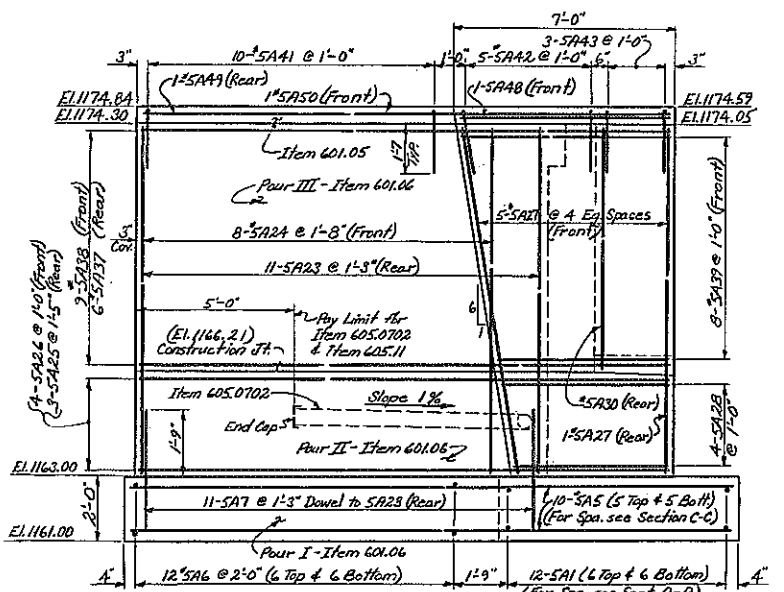
Note: For Paylimits of split faced Conc. Item 617.02 & Conc. Block Masonry Item 09617.9999 see Details Dwg. No. 22 of 27.
For Joint Details see Dwg. No. 17 & 18 of 27.
For Keyway & Waterstop Details see Dwg. No. 22 of 27.
For Design Purposes, the Foundation Pressure does not exceed 2 1/2 tons per square foot.



ELEVATION
Scale: 1/4" = 1'-0"

Steel Pipe Underdrain, 4 3/8" semi-circ or 6" Perforated Corrugated - Item 605.0702
Omit perforations in sections thru Backwall.

TABLE OF QUANTITIES			
POUR	Item 601.05	Item 601.06	Item 602.0201
Pour I Footing	---	41.37 CY	1994 #
Pour II Stem & Wingwalls	---	44.39 CY	3025 #
Pour III Backwall & W.W.s	---	40.31 CY	2,024 #
Pedestals	1.04 CY	---	104 #
Header	0.56 CY	---	91 #
N.W.W. Overlay	0.60 CY	---	40 #
N.E.W.W. Overlay	0.81 CY	---	53 #
Total	3.01 CY	126.07 CY	7331 #



NORTHWEST WINGWALL ELEVATION
Scale: 3/8" = 1'-0"

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
1 - 88
NORTH ABUTMENT PLAN & SECTION

PROJ. ENG. [Redacted] DATE MADE 7/77
SQUAD [Redacted] DRAWING NO. 10 OF 27

DESIGNED BY: [Redacted] CHECKED BY: [Redacted] DETAILED BY: [Redacted]

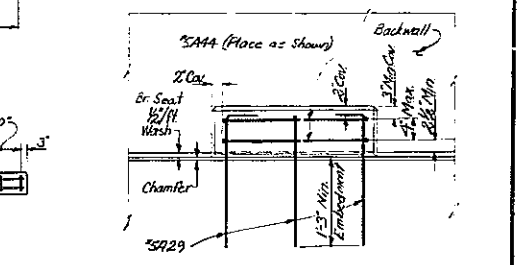
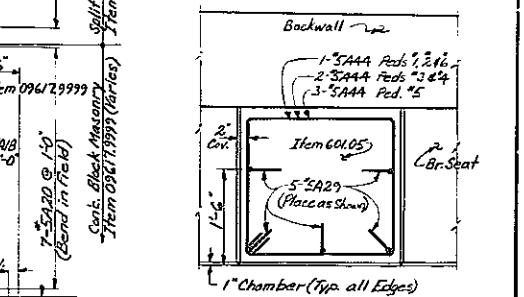
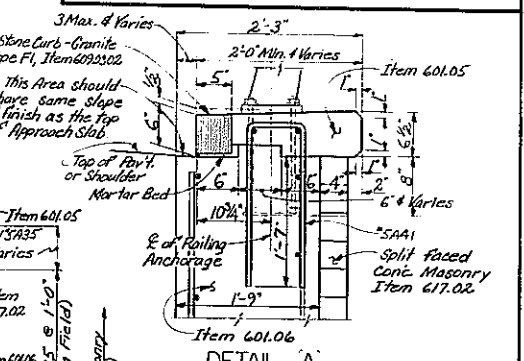
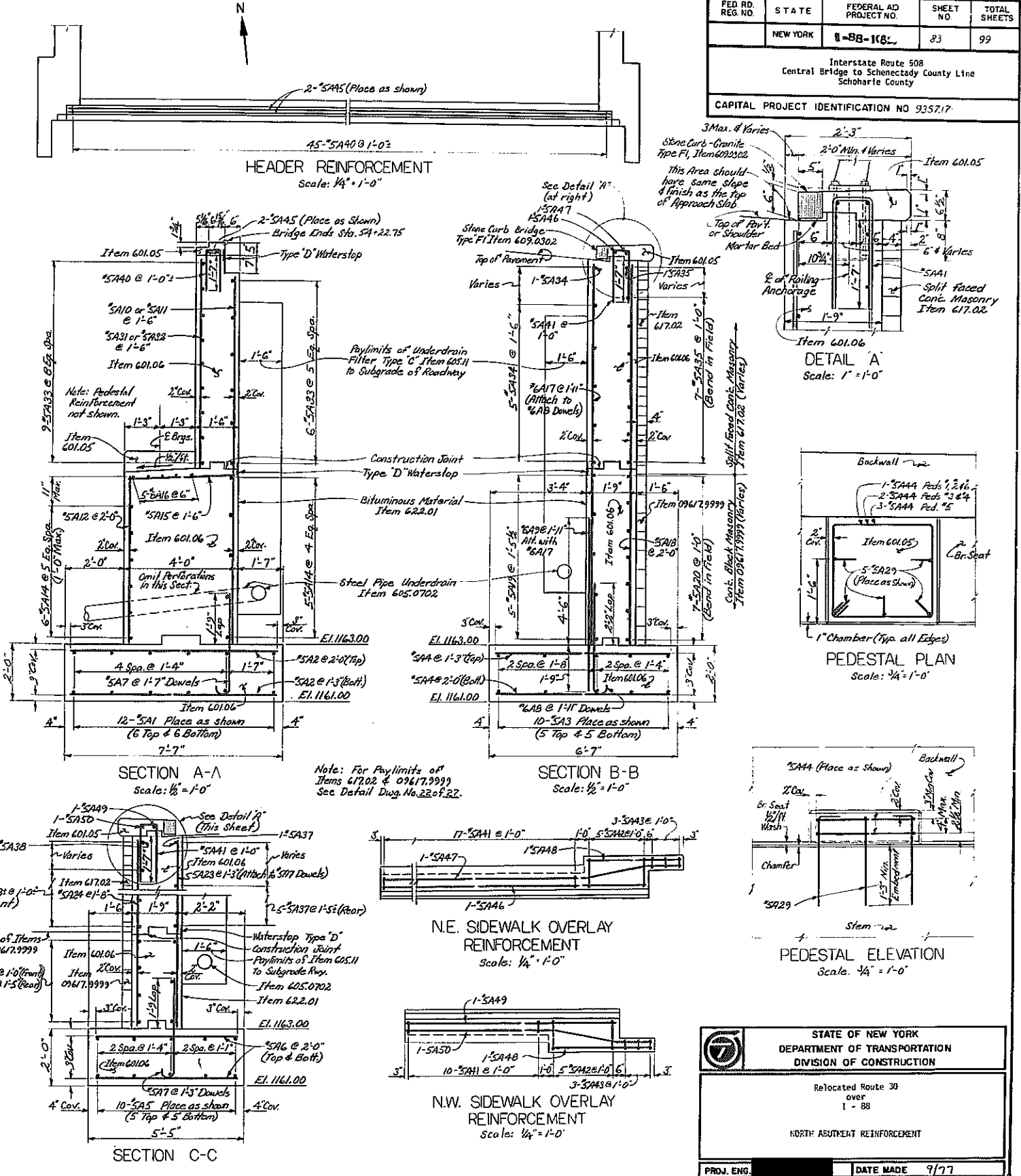
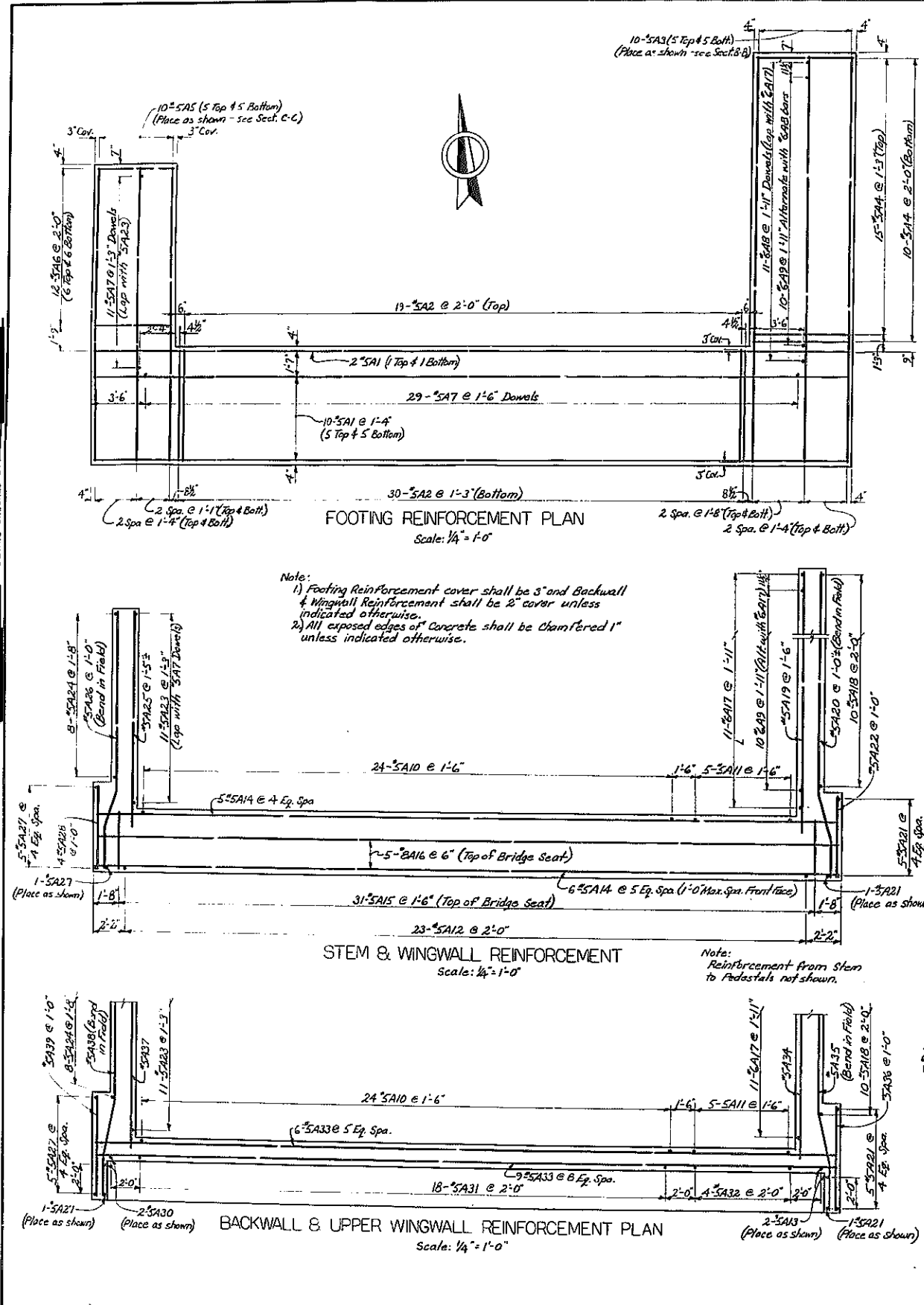


D95749

FED RD REG NO.	STATE	FEDERAL AD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	8-88-106-	83	99

Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County

CAPITAL PROJECT IDENTIFICATION NO 935717



STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
1 - 88

NORTH: ABUTMENT REINFORCEMENT

PROJ. ENG.	DATE MADE
SQUAD	DRAWING NO. 11 OF 27



D95749

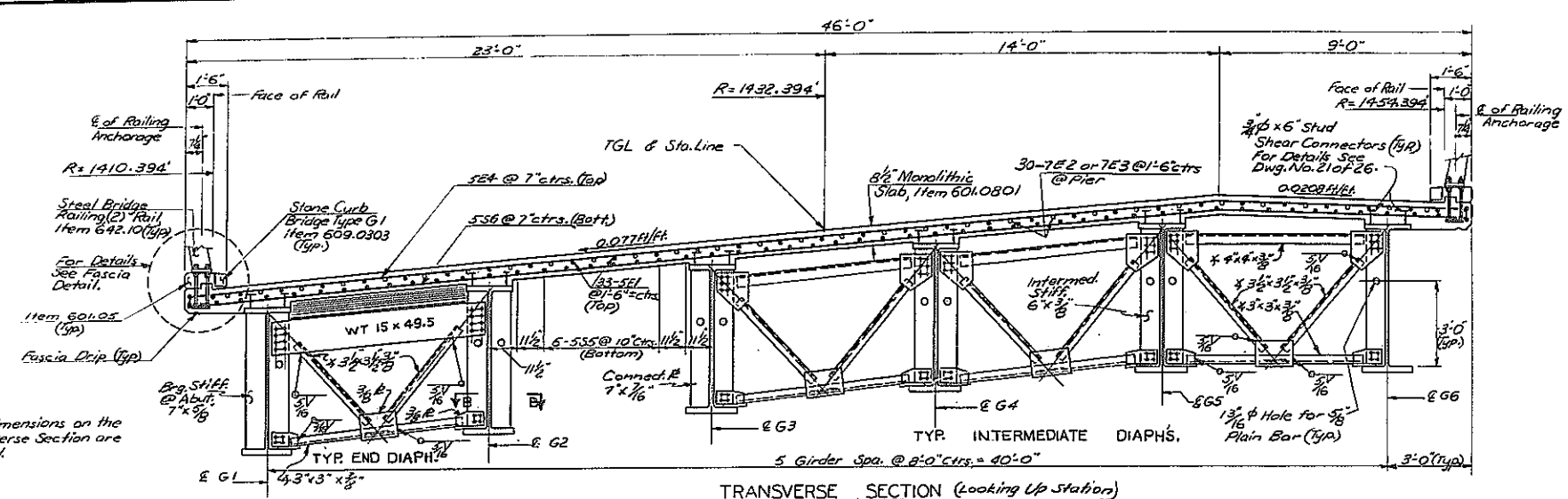
FED. RD. REG. NO.	STATE	FEDERAL AD. PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-1(E2)	84	99

Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County

CAPITAL PROJECT IDENTIFICATION NO. 935717

GIRDER	DIST. FROM E BRG @ S. ABUT.	DIST. FROM E BRG @ N. ABUT.
G1	80'-0"	25'-0"
G2	90'-0"	90'-0"
G3	90'-0"	80'-0"
G4	50'-0"	78'-0"
G5	45'-0"	68'-0"
G6	40'-0"	60'-0"

* TABLE SHOWING LOCATION OF STIRRUPS PLACED WITHIN THE DISTANCE SHOWN

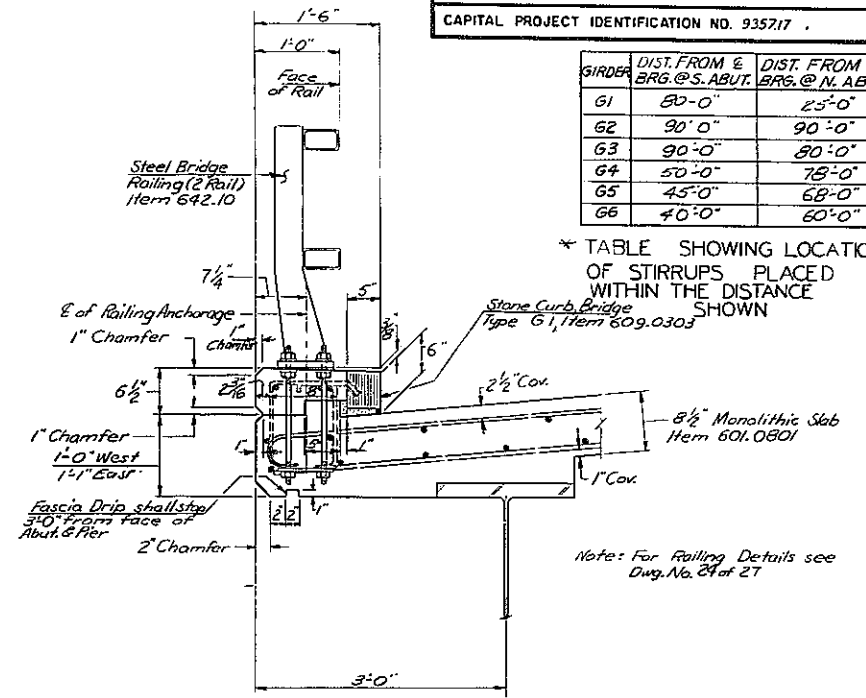


TRANSVERSE SECTION (Looking Up Station)
Scale: 3/8" = 1'-0"

Note: All dimensions on the Transverse Section are Radial.

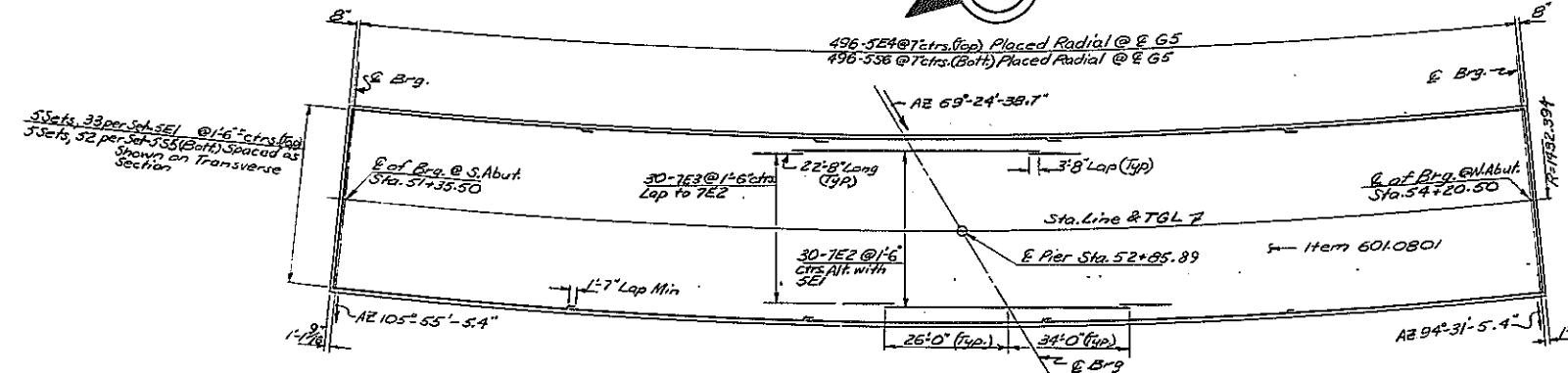
NOTES:

- Where holes are indicated, connections shall be 7/8" diameter high-strength bolts.
- Cross frames may be fabricated to fit the girders in their erected position and cambered shape, but deflected vertically under the dead load of the steel work only.
- The Contractor may place diaphragms on either side of the bearing stiffeners or stiffener connection plates as necessary to correct alignment provided there will be no interference with other structural details.
- Snipe the outstanding leg of all angles and plates 1" minimum.
- Variable thicknesses of shim plates may be used. The minimum thickness of shim plate shall be 1/8" with a maximum number of three shim plates permitted at any connection. The total thickness of all shim plates used at any connection shall not exceed 1". Shim plates shall have the dimensions of the facing surface. The shim material shall conform to ASTM Designation A36, except that on unpainted structures, the shim material shall conform to ASTM Designation A568. No additional payment will be made for furnishing and placing the shim plates.
- Both the gusset plate and flange are to be same type of steel.
- Field welding to the gusset plate will not be permitted.
- Care shall be taken when welding gusset plates to the flanges of plate girders, especially at the start and end of the fillet welds, to insure complete fusion at the root and that weld craters are properly filled. The materials to be welded shall be preheated to a minimum of 250°F.

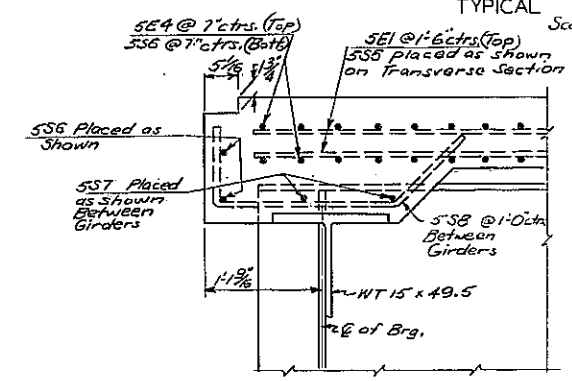


TYPICAL FASCIA DETAIL
Scale: 1" = 1'-0"

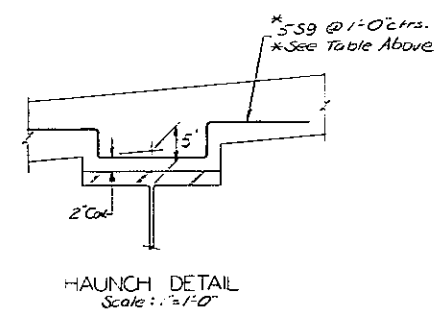
Note: For Railing Details see Dwg. No. 24 of 27



SUPERSTRUCTURE REINFORCEMENT
Scale: 1" = 20'-0"

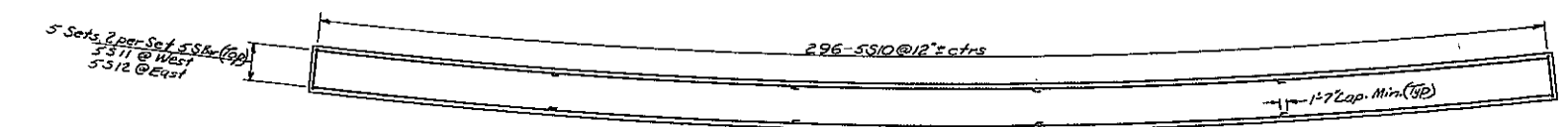


SLAB HAUNCH DETAIL AT ABUT.
Not To Scale

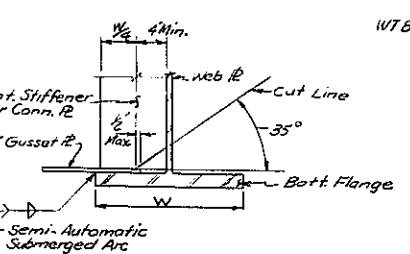


HAUNCH DETAIL
Scale: 1" = 1'-0"

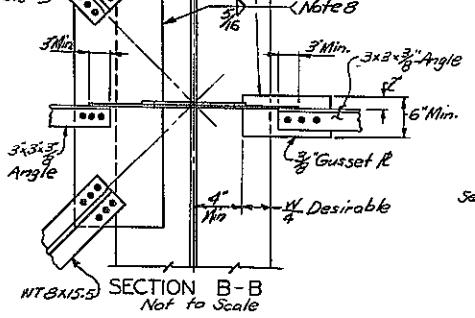
Notes: For Railing Layout see Dwg. No. 19 of 27.
For Handrail Details see Dwg. No. 21 of 27.
For Joint Details see Dwg. No. 17 of 27.
For Pouring Sequence see Dwg. No. 15 of 27.



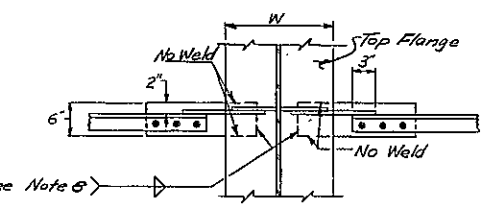
TYPICAL SAFETYWALK REINFORCEMENT
Not To Scale



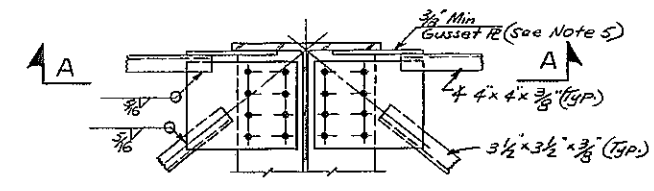
DETAIL SHOWING METHOD OF CONNECTING GUSSET PLATE AFTER STIFFENER AND/OR CONNECTION PLATE ATTACHMENT
Not To Scale



SECTION B-B
Not To Scale



SECTION A-A
Not To Scale



DETAIL OF TENSION FLANGE (TOP)
Not To Scale

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

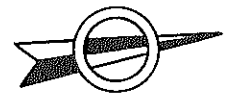
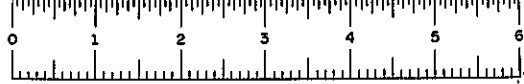
Relocated Route 30
over
I - 88

TRANSVERSE SECTION & SLAB REINFORCEMENT

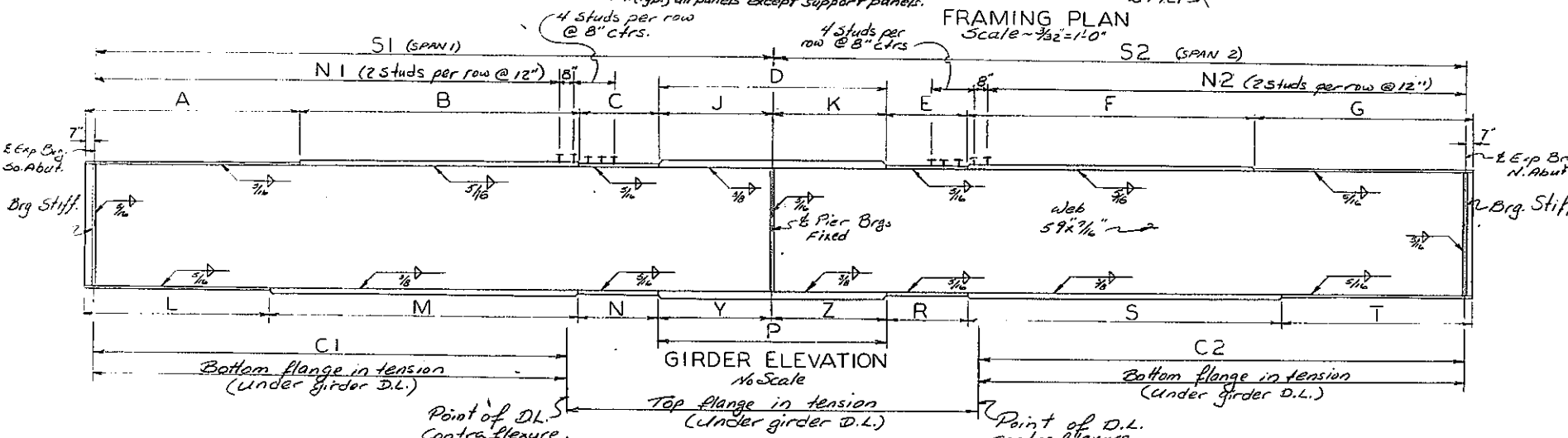
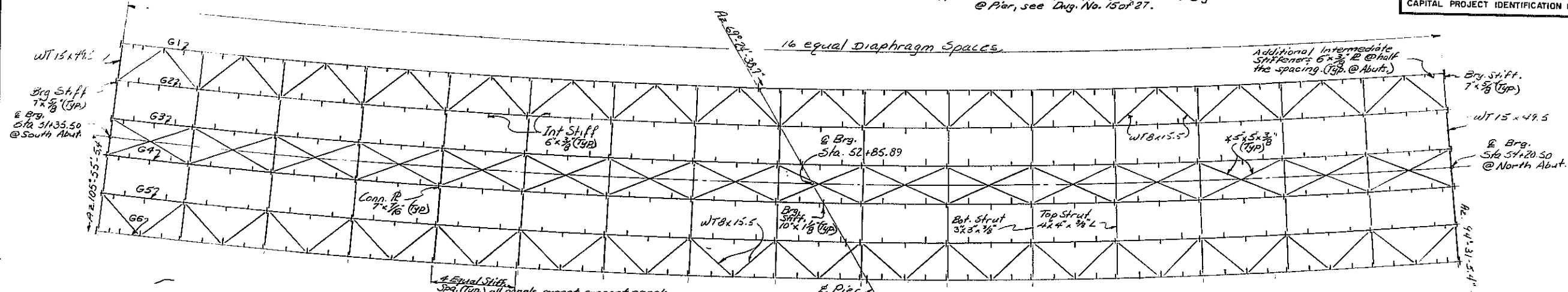
PROJ. ENG. MADE 9/77
SQUAD NO. 12 OF 27

FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	8-88-1152-	25	99

Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County
CAPITAL PROJECT IDENTIFICATION NO. 935717

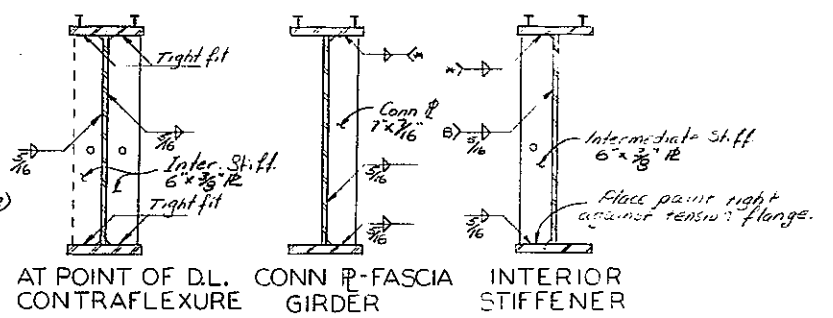
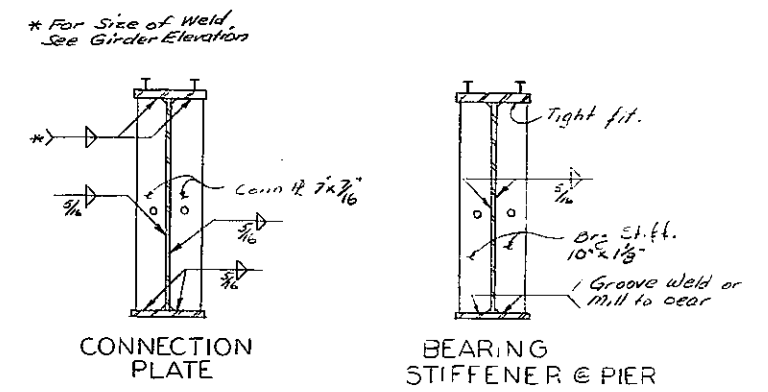


Note: For additional stiffeners near E of Brg. @ Pier, see Dwg. No. 15 of 27.



TYPE OF STEEL	ITEM 616.01
A 588	460,550*
A 36	70,250*
TOTAL	530,800*

All Flanges and Webs shall be ASTM A588 Steel
All Diaphragms, Stiffeners, Lateral Bracing & Connection R's, Gusset R's & Handrails shall be ASTM A36 Steel



GIRDER	WEB A-588	TOP FLANGE A588																BOTTOM FLANGE A588																RADIUS	SPAN LENGTHS		DIAPH. SPACING	SHEAR CONN @ 2' CTRS.		D.L. PT. OF CONTRAFLEX.	
		A	B	C	D	E	F	G	J	K	L	M	N	P	R	S	T	Y	Z	SI	S2	N1	N2	C1	C2																
G-1	282-2 1/2 59 x 7/8	80'-7" 18 x 7/8	31'-5 1/2" 18 x 1 1/2	~	45'-0" 22 x 2"	33'-6 3/8" 18 x 1 1/2	66'-0" 20 x 1 1/2	25'-7" 18 x 7/8	25'-0" 22 x 2"	20'-0" 22 x 2"	25'-7" 18 x 7/8	86'-5 1/2" 20 x 1 1/2	~	45'-0" 22 x 2"	33'-6 3/8" 20 x 1 1/2	66'-0" 20 x 1 1/2	25'-7" 18 x 7/8	25'-0" 22 x 2"	20'-0" 22 x 2"	142.394	136.48	144.54	17.56'	93	108	91.6'	106.6'														
G-2	283-1 7/8 59 x 7/8	90'-7" 18 x 7/8	32'-0 3/4" 18 x 1 1/2	~	46'-0" 22 x 2"	33'-6 3/8" 18 x 1 1/2	~	90'-7" 18 x 7/8	20'-0" 22 x 2"	20'-0" 22 x 2"	25'-7" 18 x 7/8	97'-0 3/4" 18 x 1 1/2	~	40'-0" 22 x 2"	95'-1 1/2" 18 x 1 1/2	~	25'-7" 18 x 7/8	20'-0" 22 x 2"	20'-0" 22 x 2"	142.394	142.05	140.56	17.66'	101	100	100.4'	98.9'														
G-3	285-4 1/2 59 x 7/8	90'-7" 18 x 7/8	37'-7 1/2" 18 x 1 1/2	~	45'-0" 22 x 2"	31'-7" 18 x 1 1/2	~	80'-7" 18 x 7/8	20'-0" 22 x 2"	25'-0" 22 x 2"	25'-7" 18 x 7/8	102'-7 3/8" 20 x 1 1/2	~	45'-0" 22 x 2"	88'-7" 20 x 1 1/2	~	25'-7" 18 x 7/8	20'-0" 22 x 2"	25'-0" 22 x 2"	142.394	147.61	136.59	17.76'	108	94	106.8'	92.9'														
G-4	286-1 7/8 59 x 7/8	50'-7" 18 x 7/8	83'-2 1/2" 20 x 1 1/2	~	45'-0" 22 x 2"	29'-7 1/2" 20 x 1 1/2	~	78'-7" 18 x 7/8	20'-0" 22 x 2"	25'-0" 22 x 2"	25'-7" 18 x 7/8	108'-2 1/2" 20 x 1 1/2	~	45'-0" 22 x 2"	82'-7 1/2" 20 x 1 1/2	~	25'-7" 18 x 7/8	20'-0" 22 x 2"	25'-0" 22 x 2"	142.394	153.16	132.63	17.86'	114	87	113.1'	85.8'														
G-5	288-3 3/8 59 x 7/8	45'-7" 18 x 7/8	53'-6 3/8" 20 x 1 1/2	~	45'-0" 22 x 2"	35'-8 3/8" 18 x 1 1/2	~	68'-7" 18 x 7/8	20'-0" 22 x 2"	25'-0" 22 x 2"	25'-7" 18 x 7/8	83'-8 3/8" 20 x 1 1/2	30'-0" 22 x 2"	45'-0" 22 x 2"	78'-8 3/8" 18 x 1 1/2	~	25'-7" 18 x 7/8	20'-0" 22 x 2"	25'-0" 22 x 2"	144.394	158.71	128.68	17.96'	123	81	120.0'	80.3'														
G-6	290-1 7/8 59 x 7/8	46'-7" 18 x 7/8	64'-3" 20 x 1 1/2	~	45'-0" 22 x 2"	39'-8 3/8" 18 x 1 1/2	~	60'-7" 18 x 7/8	20'-0" 22 x 2"	25'-0" 22 x 2"	25'-7" 18 x 7/8	84'-3" 20 x 1 1/2	30'-0" 22 x 2"	45'-0" 22 x 2"	74'-8 3/8" 18 x 1 1/2	~	25'-7" 18 x 7/8	20'-0" 22 x 2"	25'-0" 22 x 2"	142.394	164.24	124.74	18.06'	128	73	126.7'	72.1'														

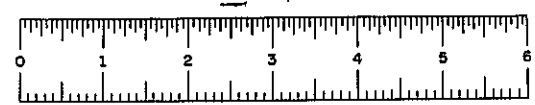
LATERAL BRACING @ MIDDLE BAY
Not To Scale

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I - 88

FRAMING PLAN & GIRDER DETAILS

PROJ. SQUAD DATE MADE 9/77
DRAWING NO. 13 OF 27



D95749

FED. RD. REG. NO.	STATE	FEDERAL AD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-(152)	86	99
Interstate Route 508 Central Bridge to Schoenectady County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				

D.L. = Dead Load
S.D.L. = Superimposed Dead Load
L.L. = Live Load

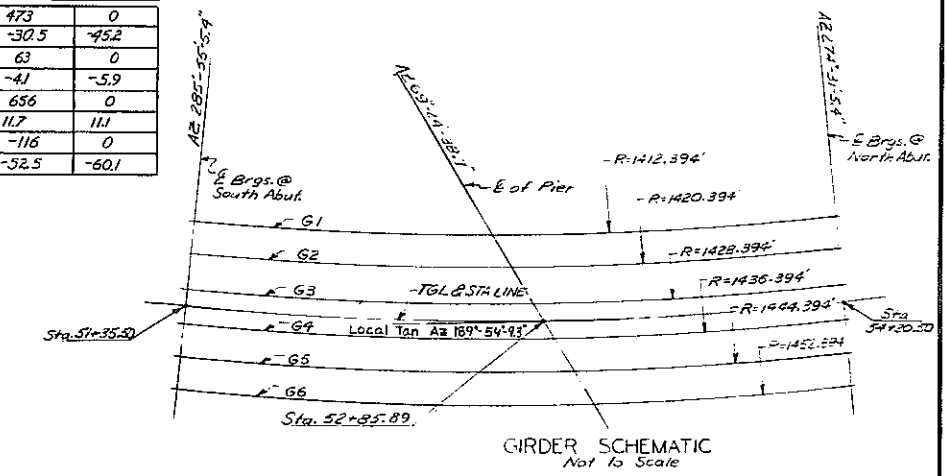
L.L. Moments and Shears include Impact - both spans.
Moments are expressed as Foot Kips.
Shears are expressed as Kips.

MOMENT & SHEAR TABLE

GIRDER	Type	BRGS. South ABUT.	SPAN 1																		BRGS. North ABUT.		
			O.1L ₁	O.2L ₁	O.3L ₁	O.4L ₁	O.5L ₁	O.6L ₁	O.7L ₁	O.8L ₁	O.9L ₁	PIER	O.1L ₂	O.2L ₂	O.3L ₂	O.4L ₂	O.5L ₂	O.6L ₂	O.7L ₂	O.8L ₂		O.9L ₂	
1	D.L. Moment	0	651	1084	1296	1284	1048	589	-99	-1012	-2159	-3566	-2013	-744	265	1019	1509	1734	1694	1387	821	0	
	D.L. Shear	56.0	39.9	23.7	7.3	-91	-25.6	-42.1	-58.7	-75.4	-93.9	-112.6	-117.1	97.1	78.6	60.8	43.1	24.7	6.4	-12.0	-30.3	-47.9	-65.1
	S.D.L. Moment	0	80	133	159	158	131	76	-5	-113	-248	-410	-232	-84	33	119	176	201	197	162	96	0	
2	D.L. Moment	0	817	1377	1672	1702	1467	966	198	-840	-2151	-3755	-2311	1124	-180	530	1011	1264	1287	1083	652	0	
	D.L. Shear	59.7	43.0	26.2	9.1	-8.1	-25.2	42.3	-59.5	-76.9	-95.1	-114.5	-114.1	94.6	76.7	59.5	42.4	25.4	8.3	-8.6	-25.7	-42.4	-59.2
	S.D.L. Moment	0	94	158	193	200	177	125	44	-66	-206	-374	-207	-69	41	121	173	196	190	156	92	0	
3	D.L. Moment	0	908	1539	1886	1945	1715	1192	378	-726	-2125	-3842	-2444	1291	-370	329	811	1077	1128	964	587	0	
	D.L. Shear	65.5	50.4	32.1	13.4	-5.6	-24.7	43.7	-62.8	-81.9	-101.8	-123.1	-114.5	95.9	77.6	61.0	44.3	28.1	12.0	-4.3	-20.5	-36.2	-52.0
	S.D.L. Moment	0	103	175	216	225	203	149	64	-88	-201	-380	-220	-86	20	98	150	174	172	142	84	0	
4	D.L. Moment	0	1023	1749	2166	2264	2041	1496	632	-545	-2033	-3864	-2515	1398	-501	185	663	935	1005	870	534	0	
	D.L. Shear	74.0	55.3	36.2	16.4	-4.0	-24.2	44.6	-64.6	-84.1	-104.5	-126.7	-113.5	95.5	77.7	61.4	45.2	29.0	13.2	-2.6	-18.4	-33.7	-49.0
	S.D.L. Moment	0	126	212	264	279	257	200	106	24	-191	-394	-246	-122	22	54	105	132	136	118	71	0	
5	D.L. Moment	0	1136	1948	2423	2552	2335	1770	861	-378	-1949	-3878	-2590	1521	-656	13	488	769	860	761	473	0	
	D.L. Shear	79.1	59.6	39.3	18.5	-2.7	-23.9	45.1	-65.6	-85.8	-106.2	-129.2	-111.6	94.1	77.0	61.3	45.8	30.1	14.9	-0.4	-15.6	-30.5	-45.2
	S.D.L. Moment	0	136	232	290	309	289	230	132	25	-181	-396	-254	-135	39	86	114	120	102	63	0	0	
6	D.L. Moment	0	1171	1992	2513	2642	2425	1860	951	-400	-2000	-3909	-2610	1560	-700	200	700	1000	1100	950	600	0	
	D.L. Shear	81.0	60.0	40.0	20.0	-5.0	-25.0	45.0	-65.0	-85.0	-105.0	-125.0	-115.0	95.0	75.0	60.0	45.0	30.0	15.0	-0.5	-16.0	-31.0	-46.0
	S.D.L. Moment	0	150	250	310	325	300	230	140	30	-200	-400	-250	-150	50	100	150	170	160	130	80	0	

THEORETICAL BOTTOM OF SLAB ELEVATION

GIRDER	SPAN 1										PIER	SPAN 2										BRGS. North ABUT.
	1-1	1-2	1-3	1-4	1-5	1-6	1-7	1-8	1-9	1-10		2-1	2-2	2-3	2-4	2-5	2-6	2-7	2-8	2-9	2-10	
1	1167.67	1167.85	1170.04	1170.22	1170.41	1170.59	1170.78	1170.96	1171.15	1171.33	1171.52	1171.71	1171.91	1172.11	1172.30	1172.50	1172.70	1172.90	1173.09	1173.29	1173.49	
2	1170.28	1170.47	1170.67	1170.86	1171.05	1171.24	1171.43	1171.62	1171.81	1172.01	1172.20	1172.39	1172.58	1172.77	1172.96	1173.15	1173.34	1173.53	1173.72	1173.91	1174.10	
3	1170.90	1171.10	1171.30	1171.49	1171.69	1171.89	1172.09	1172.28	1172.48	1172.68	1172.88	1173.06	1173.25	1173.43	1173.61	1173.80	1173.98	1174.17	1174.35	1174.53	1174.72	
4	1171.52	1171.72	1171.92	1172.13	1172.33	1172.54	1172.74	1172.94	1173.15	1173.35	1173.56	1173.73	1173.91	1174.09	1174.27	1174.45	1174.62	1174.80	1174.98	1175.16	1175.33	
5	1172.13	1172.34	1172.55	1172.76	1172.97	1173.18	1173.39	1173.60	1173.81	1174.02	1174.24	1174.41	1174.58	1174.75	1174.92	1175.09	1175.26	1175.44	1175.61	1175.78	1175.95	
6	1172.76	1172.98	1173.20	1173.43	1173.66	1173.89	1174.11	1174.33	1174.55	1174.77	1174.99	1175.21	1175.43	1175.65	1175.87	1176.09	1176.31	1176.53	1176.75	1176.97	1177.19	



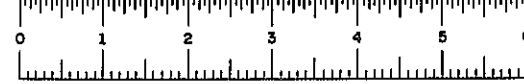
DESIGNED BY: [REDACTED] CHECKED BY: [REDACTED] DETAILED BY: [REDACTED]

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocate: Route 30
over
1 - 55'

MOMENTS, SHEAR & BOTTOM OF SLAB

PROJ. ENG. [REDACTED] DATE MADE 9/77
SQUAD [REDACTED] DRAWING NO. 14 OF 27

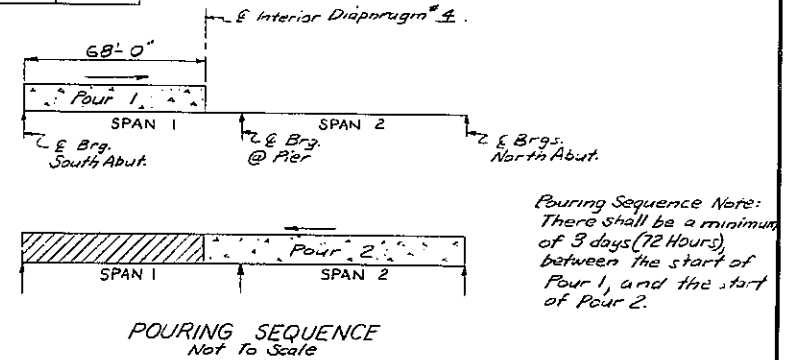
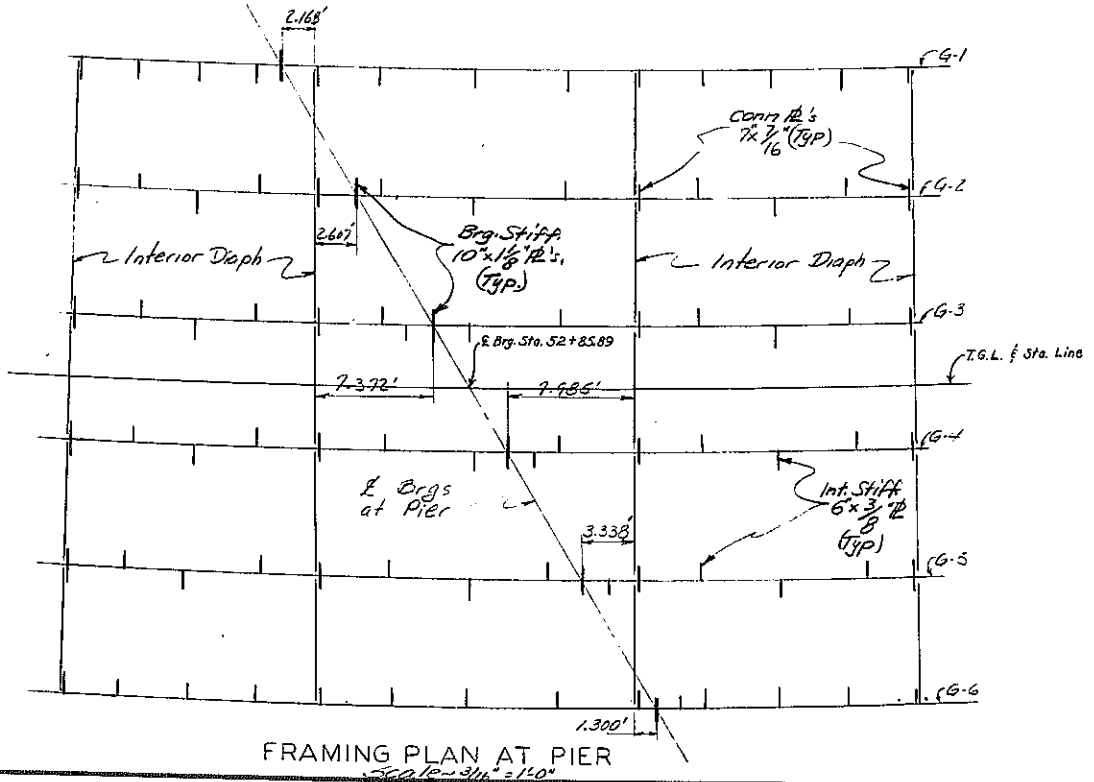
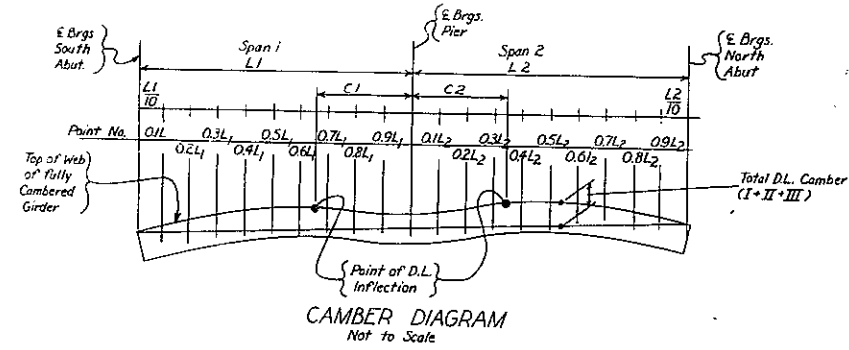


D95749

FED. RD. REG. NO.	STATE	FEDERAL RD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-1(62)	87	99
Interstate Route 508 Central Bridge to Schoharie County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				

CAMBER TABLE

GIRDERS	C. BRGS. SOUTH ABUT.	C. BRGS. PIER									C. BRGS. NORTH ABUT.											
		0.1L ₁	0.2L ₁	0.3L ₁	0.4L ₁	0.5L ₁	0.6L ₁	0.7L ₁	0.8L ₁	0.9L ₁	0.1L ₂	0.2L ₂	0.3L ₂	0.4L ₂	0.5L ₂	0.6L ₂	0.7L ₂	0.8L ₂	0.9L ₂			
1	Steel D.L. (ft.)	0.000	.014	.025	.031	.033	.030	.023	.014	.006	-.001	.000	.006	.017	.031	.044	.052	.054	.049	.038	.021	.000
	Concrete D.L. (ft.)	0.000	-.083	-.152	-.198	-.216	-.206	-.172	-.119	-.067	-.024	0.000	-.004	-.033	-.070	-.115	-.146	-.158	-.153	-.116	-.067	0.000
	Superimposed D.L. (ft.)	.000	.006	.011	.013	.014	.013	.010	.006	.002	-.000	.000	.004	.010	.017	.024	.028	.029	.027	.021	.012	.000
	Total D.L. ft.																					
Vertical Curve (ft.)		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
2	Steel D.L. (ft.)	.000	.019	.034	.043	.047	.044	.036	.024	.012	.004	.000	.003	.010	.021	.033	.041	.044	.041	.032	.018	.000
	Concrete D.L. (ft.)	0.000	-.101	-.183	-.239	-.259	-.250	-.213	-.139	-.088	-.033	0.000	.006	-.014	-.052	-.095	-.125	-.140	-.141	-.111	-.061	0.000
	Superimposed D.L. (ft.)	.000	.009	.016	.020	.022	.021	.017	.012	.006	.002	.000	.002	.006	.011	.017	.020	.021	.019	.015	.008	.000
	Total D.L. ft.																					
Vertical Curve (ft.)		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
3	Steel D.L. (ft.)	.000	.023	.042	.055	.060	.057	.047	.033	.018	.006	.000	.001	.007	.015	.024	.032	.035	.033	.026	.014	.000
	Concrete D.L. (ft.)	-.0000	-.118	-.214	-.278	-.303	-.295	-.252	-.183	-.109	-.043	0.000	.009	-.002	-.036	-.068	-.101	-.101	-.085	-.063	-.04	0.000
	Superimposed D.L. (ft.)	.000	.010	.018	.023	.025	.024	.021	.015	.008	.003	.000	.001	.004	.008	.012	.015	.016	.015	.012	.007	.000
	Total D.L. ft.																					
Vertical Curve (ft.)		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4	Steel D.L. (ft.)	.000	.028	.051	.066	.072	.069	.059	.043	.025	.010	.000	-.002	.001	.007	.015	.021	.024	.024	.019	.011	.000
	Concrete D.L. (ft.)	0.000	-.131	-.245	-.321	-.349	-.343	-.292	-.216	-.130	-.054	0.000	.016	.009	-.016	-.044	-.070	-.090	-.093	-.073	-.045	0.000
	Superimposed D.L. (ft.)	.000	.012	.021	.027	.030	.029	.025	.019	.011	.004	.000	-.000	.002	.005	.009	.011	.012	.012	.009	.005	.000
	Total D.L. ft.																					
Vertical Curve (ft.)		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5	Steel D.L. (ft.)	.000	.034	.062	.079	.086	.084	.072	.054	.032	.013	.000	-.004	.003	.001	.007	.013	.016	.017	.014	.008	.000
	Concrete D.L. (ft.)	0.000	-.150	-.276	-.365	-.399	-.392	-.336	-.253	-.151	-.054	0.000	.020	.019	.005	-.015	-.044	-.061	-.067	-.058	-.036	0.000
	Superimposed D.L. (ft.)	.000	.013	.023	.030	.033	.033	.028	.022	.013	.005	.000	-.001	.000	.003	.006	.008	.009	.009	.007	.004	.000
	Total D.L. ft.																					
Vertical Curve (ft.)		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6	Steel D.L. (ft.)	.000	.040	.072	.093	.103	.101	.088	.067	.041	.017	.000	.006	.008	.005	.001	.004	.008	.009	.009	.005	.000
	Concrete D.L. (ft.)	0.000	-.173	-.314	-.414	-.455	-.450	-.383	-.290	-.171	-.063	0.000	.030	.035	.007	-.017	-.034	-.043	-.038	-.027	0.000	
	Superimposed D.L. (ft.)	.000	.014	.026	.035	.039	.039	.034	.026	.016	.006	.000	.002	.001	.001	.003	.005	.007	.007	.006	.003	.000
	Total D.L. ft.																					
Vertical Curve (ft.)		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0



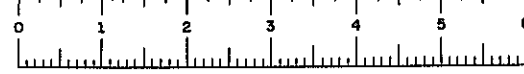
DESIGNED BY: [Redacted] CHECKED BY: [Redacted] DATE: 9/77

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

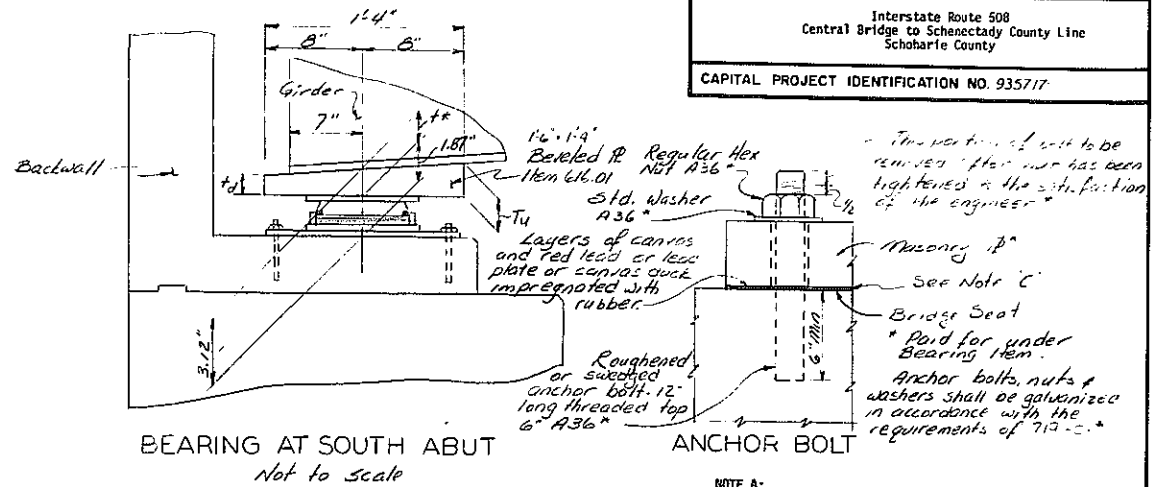
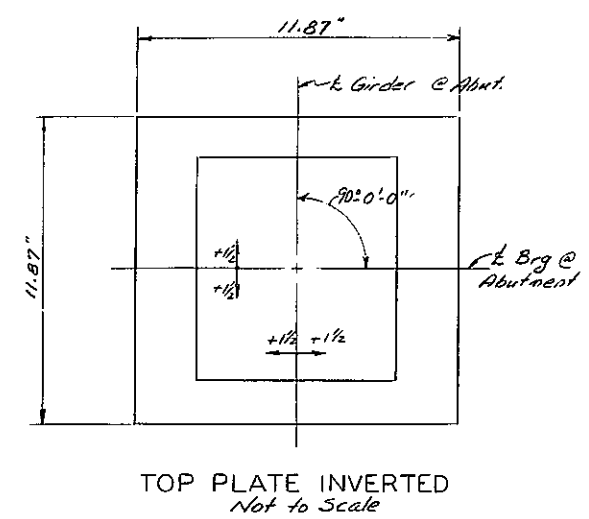
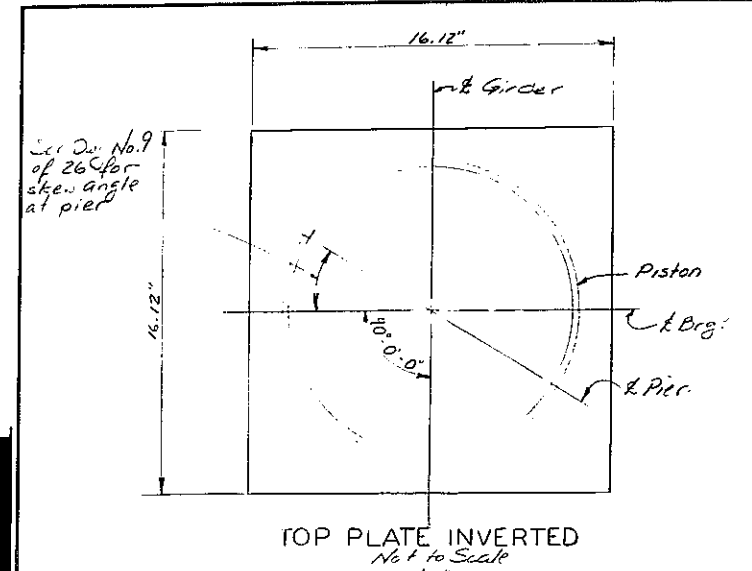
Relocated Route 30
over
I - 88

CAMBERS & POUR SEQUENCE

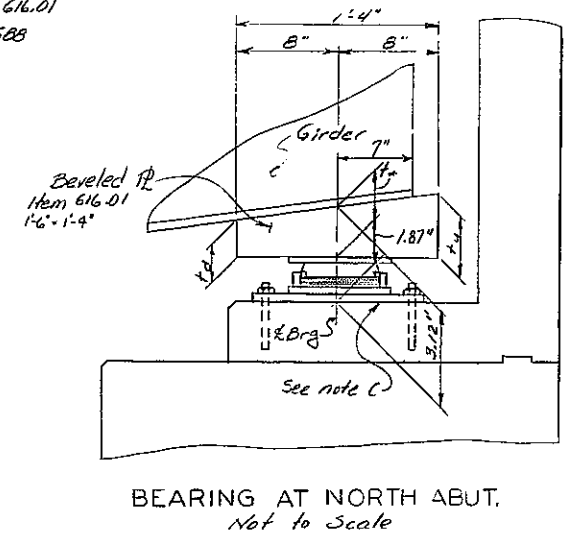
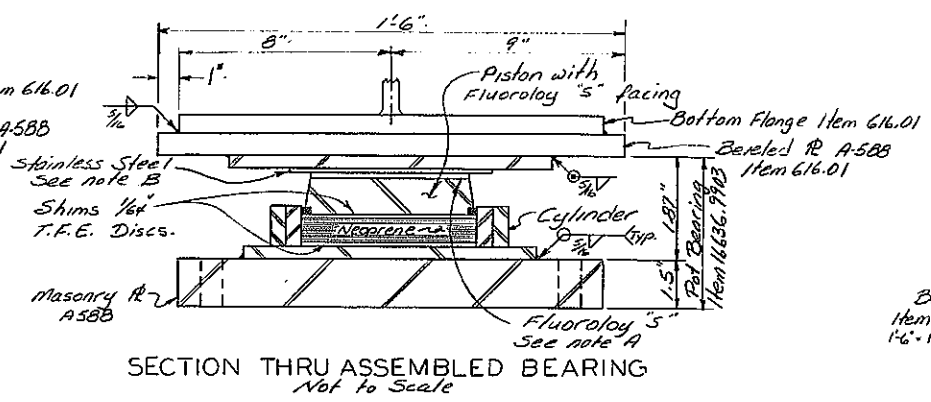
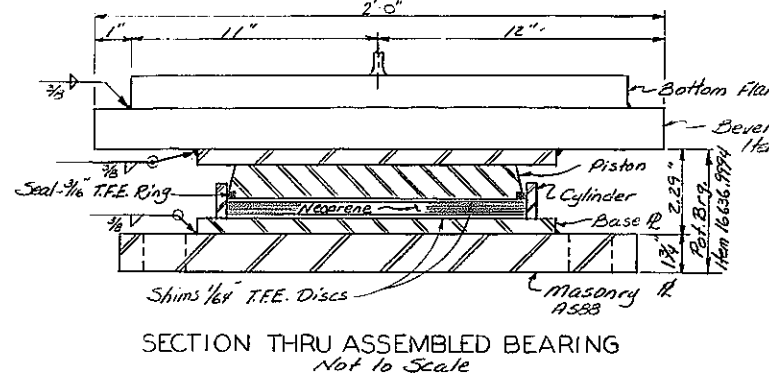
PROJ. [Redacted] SQUAD [Redacted] DATE MADE 9/77 DRAWING NO. 15 OF 27



FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	88-1152	88	99
Interstate Route 508 Central Bridge to Schenectady County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				

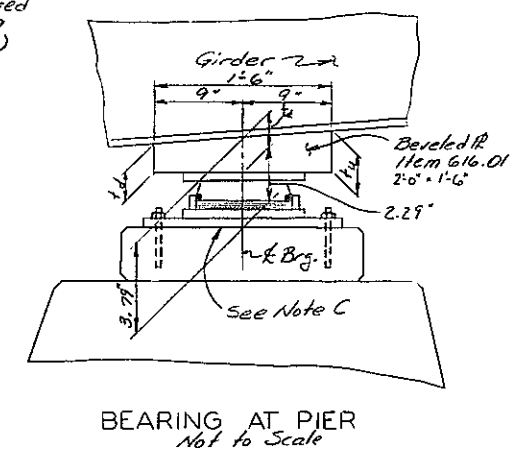
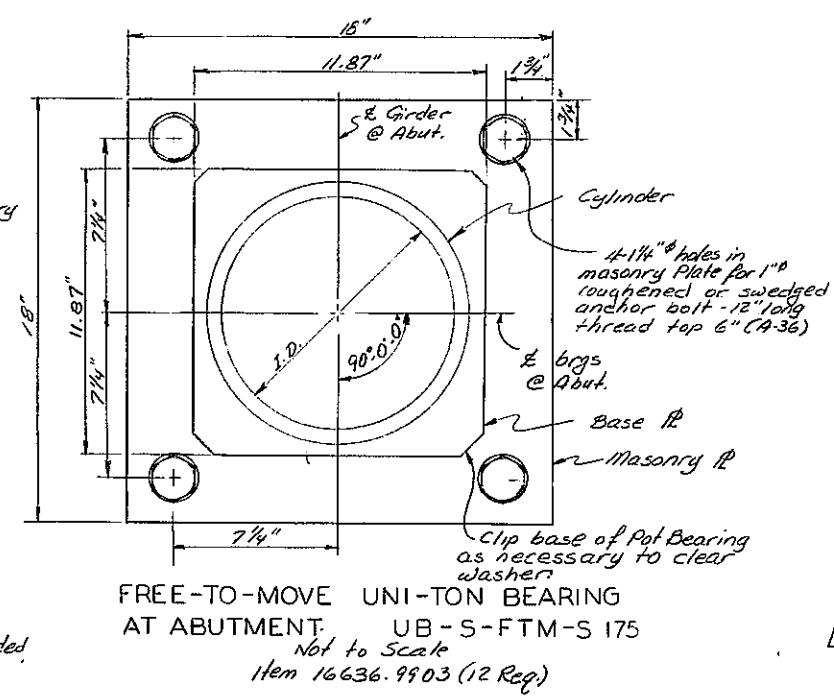
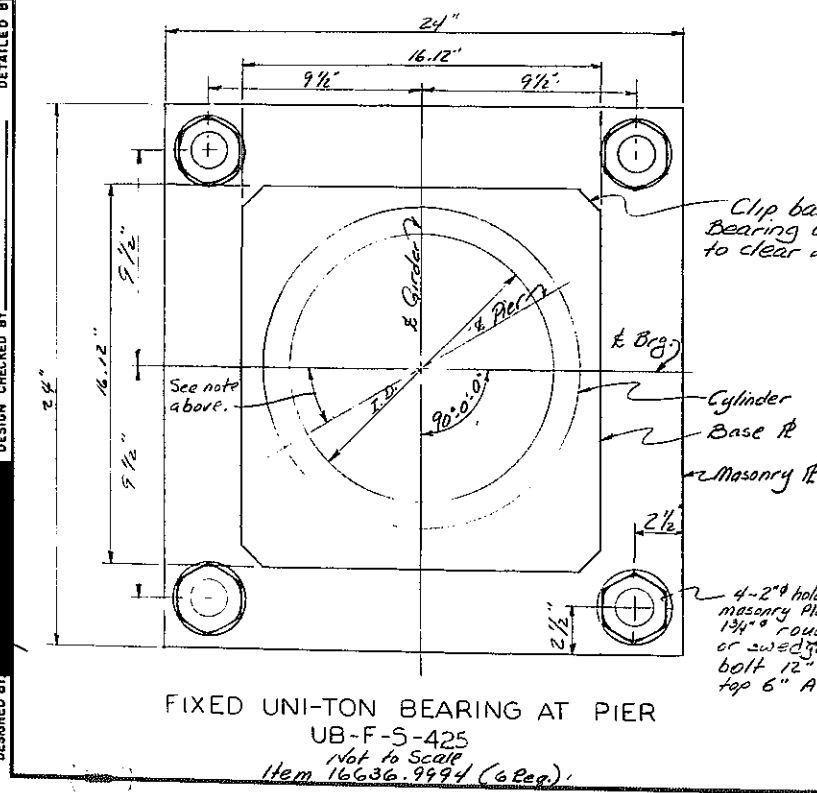


- NOTE A:**
The piston and the fluoroloy "S" (P.T.F.E.) facing shall be attached in such a manner so as to be able to withstand 20% of the applied bearing load in the shear plane.
- NOTE B:**
The stainless steel plate shall be attached to the "Top Plate" in such a manner so as to be able to withstand 20% of the applied bearing load in the shear plane. Stainless steel shall be passivated.
- NOTE C:**
Bearings shall be installed in a truly level condition. It may be necessary to grind concrete under the bearing masonry plate to accomplish this. Any grinding that is necessary shall be paid for under item 601.05.



- NOTES:**
- All steel in the bearing shall be ASTM A36.
 - The expansion bearings shall be set concentrically under full permanent Dead Load with the Ambient air temperature between 40° and 50° F.
 - Bearings must be supported completely over the entire area of the masonry and top plates. Full contact bearing must be achieved.
 - The temperature of the elastomeric disc during welding shall be limited to 150° F. using suitable procedures.
 - The bearing manufacturer shall be responsible for the manufacture of all parts containing P.T.F.E. Sliding surfaces and interfacing stainless steel surfaces. The bearing shall be shipped as an assembled unit. The bearings are to be precompressed to 500 psi.
 - All fabrication and welding procedures shall conform to the New York State Steel Construction Manual with current addenda.

PLATE SIZE	BEVELED PLATE THICKNESS (in.)		
	N. ABUT	PIER	S. ABUT.
16x18	1.1428	1.3774	1.1428
Thickness Down Sta. Side t _d	1.3572	1.6206	1.3572
*Thickness @ K. Brgs t _k	1.2500	1.5000	1.2500



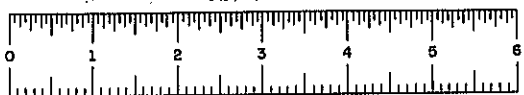
STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I - 88

UNI-TON BEARING DETAILS

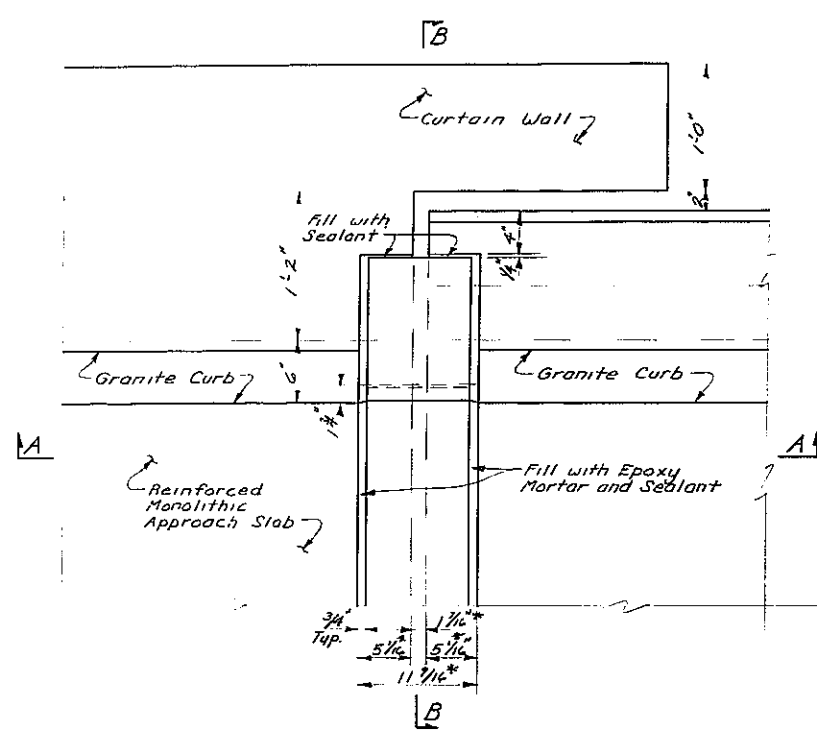
PROJ. SQUAD	DATE MADE 9/77
	DRAWING NO. 16 OF 27

DESIGNED BY: _____ CHECKED BY: _____

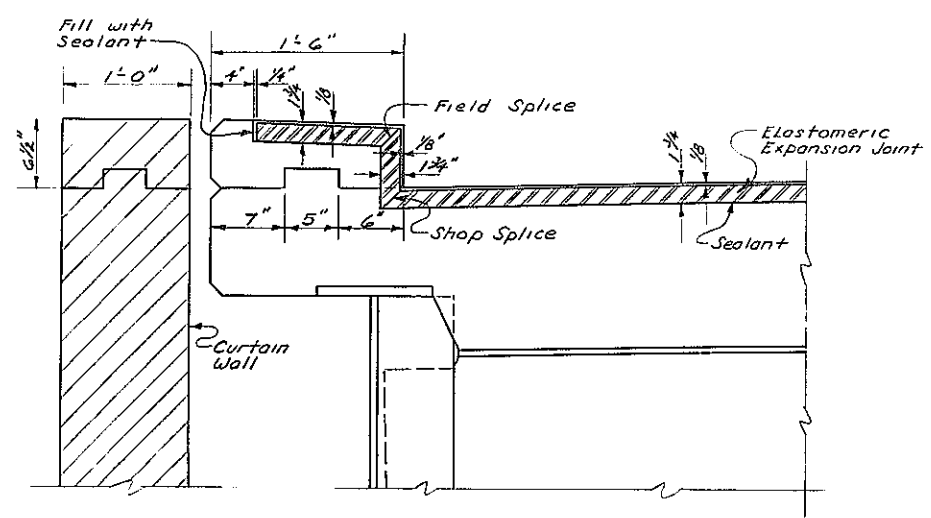


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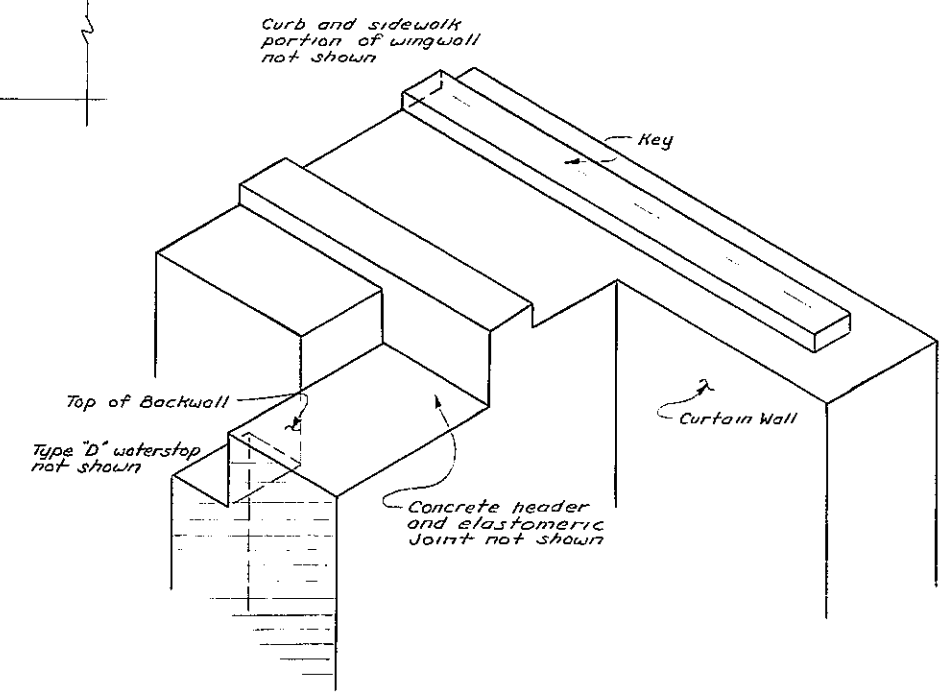
FED. RD. REG. NO.	STATE	FEDERAL AD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	88-1(52)	89	99
Interstate Route 508 Central Bridge to Schoharie County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				



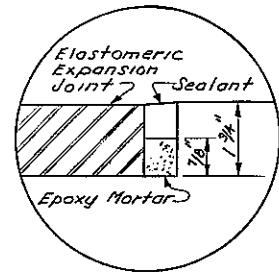
JOINT PLAN
No Scale



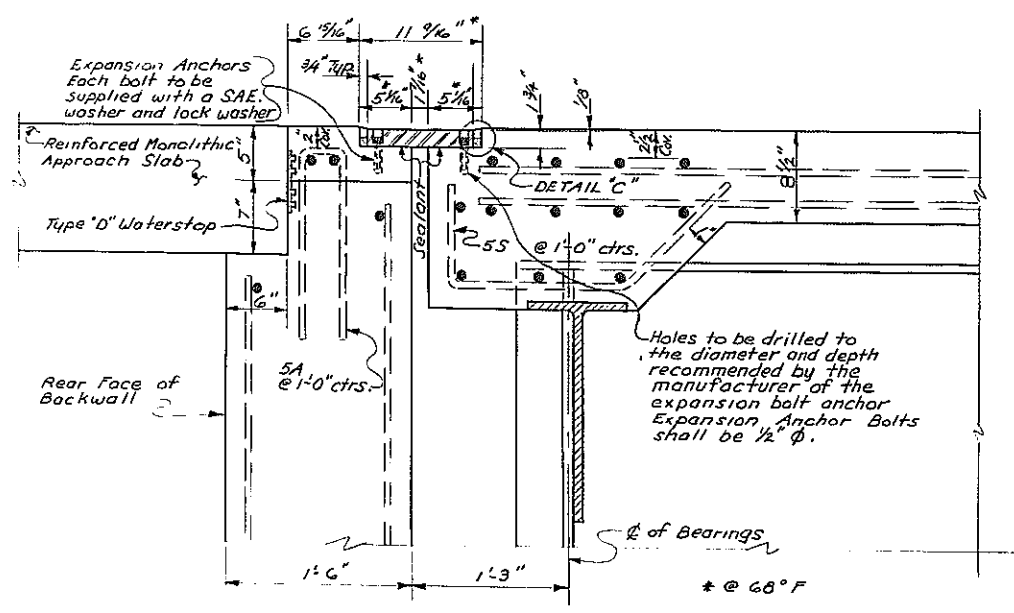
SECTION B-B
No Scale



PARTIAL ISOMETRIC of ABUTMENT
No Scale



DETAIL "C"
No Scale



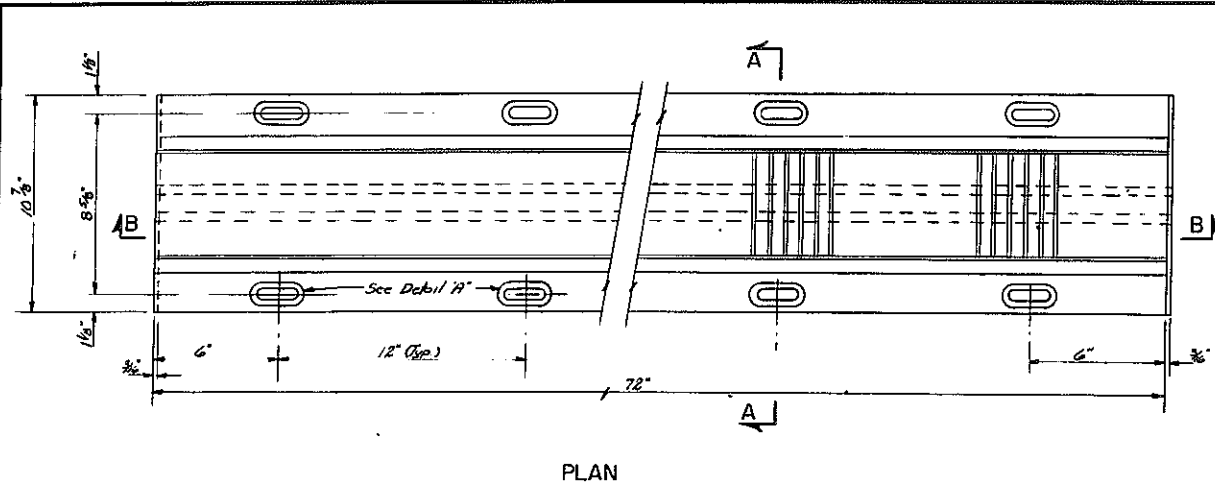
SECTION A-A
No Scale

DESIGNED BY: [REDACTED] CHECKED BY: [REDACTED] DETAILED BY: [REDACTED]

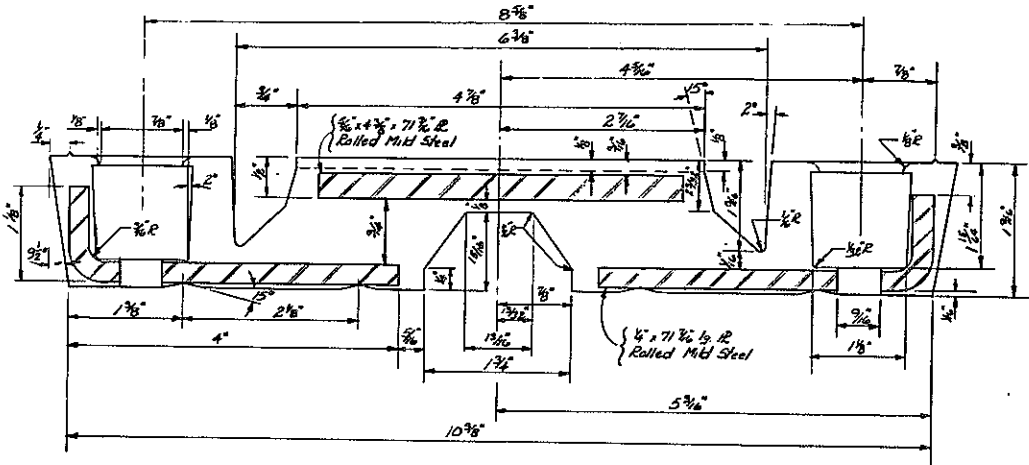
STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION DIVISION OF CONSTRUCTION	
Relocated Route 30 over I - 88	
ELASTOMERIC EXPANSION JOINT DETAILS	
PROJ. ENG. 4	DATE MADE 9/77
SQUAD 2.1	DRAWING NO. 17 OF 27

FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-(157)	90	99

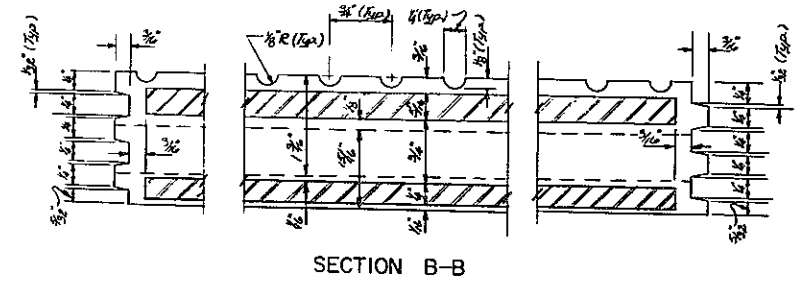
Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County
CAPITAL PROJECT IDENTIFICATION NO. 935717



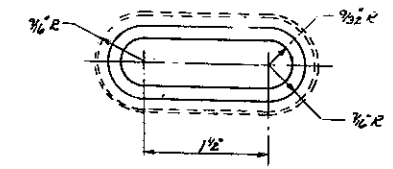
PLAN



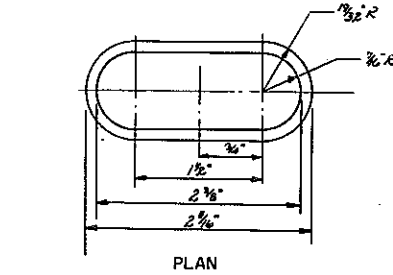
SECTION A-A



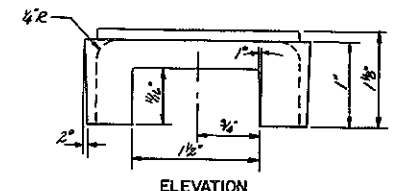
SECTION B-B



DETAIL "A"

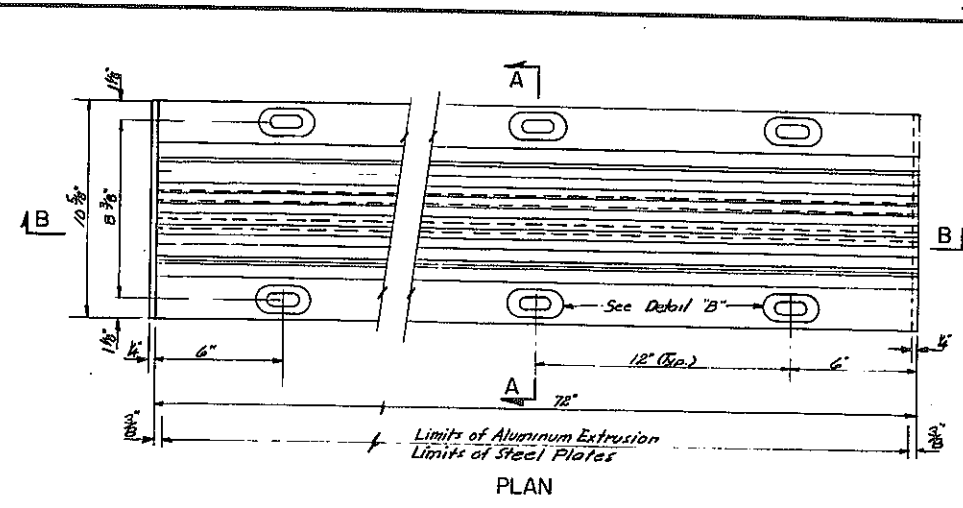


PLAN

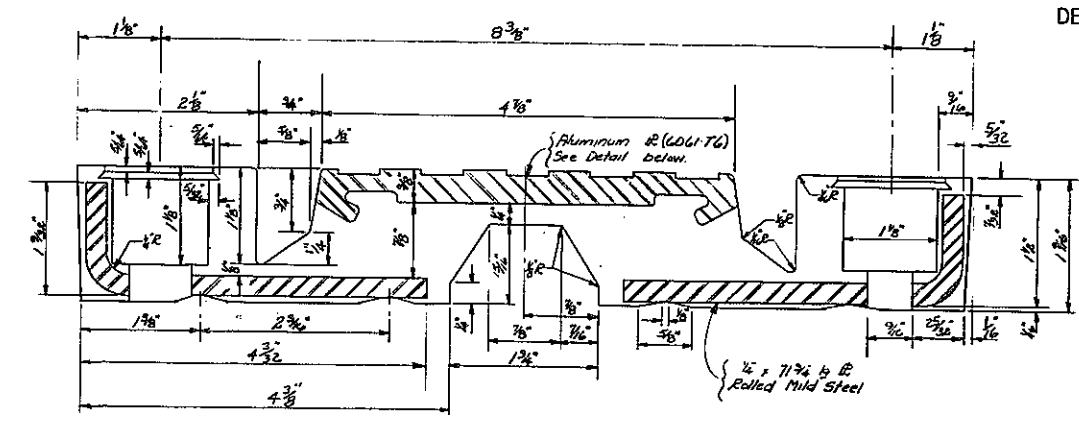


ELEVATION

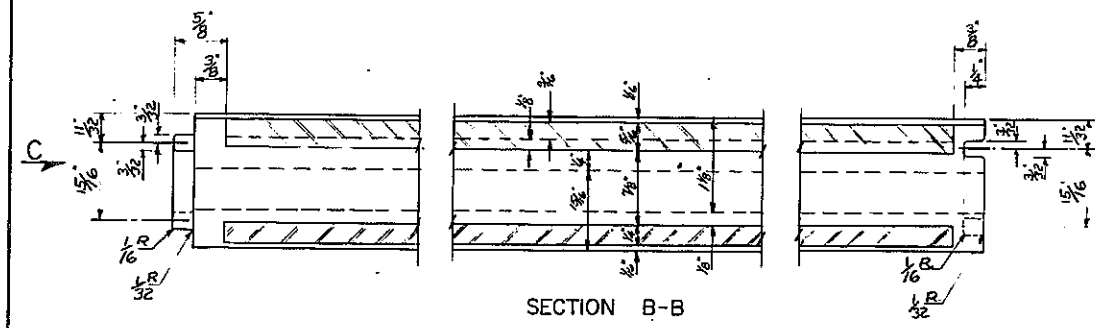
DETAIL OF CAP PLUG
TYPE 200A



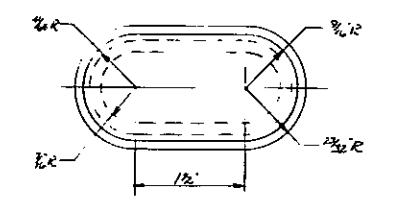
PLAN



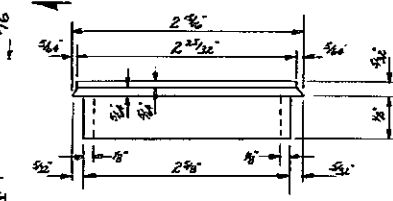
SECTION A-A



SECTION B-B

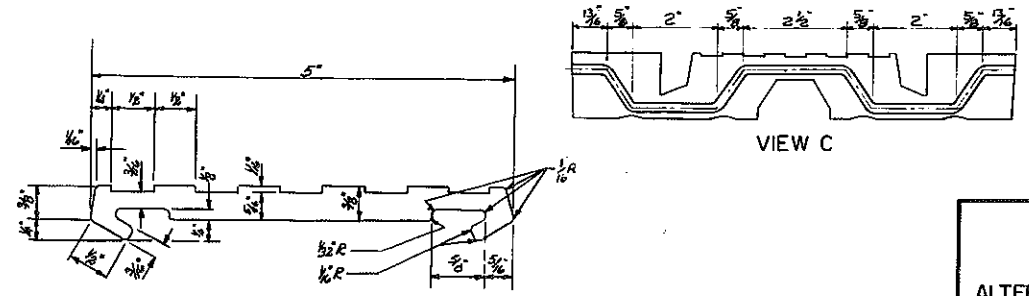


PLAN



ELEVATION

DETAIL OF CAP PLUG
TYPE 200B

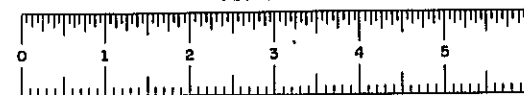


VIEW C

DETAIL OF ALUMINUM PLATE

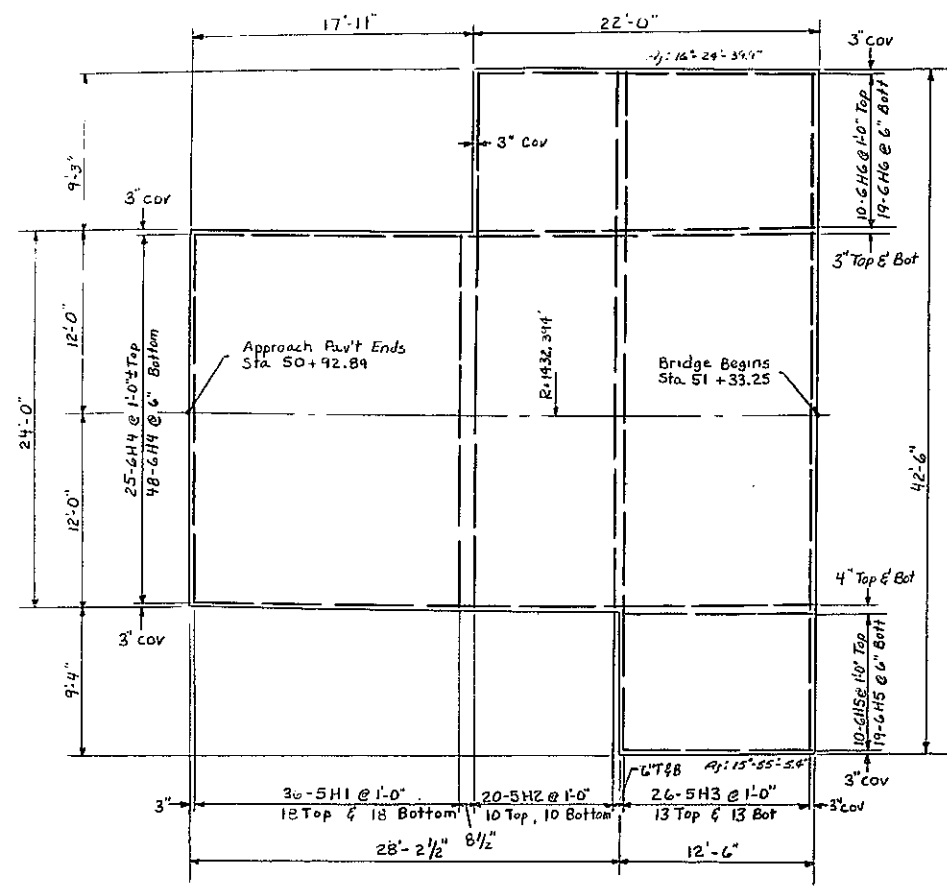
DETAILED BY: *VWCF*
DETAIL CHECKED BY: *[Signature]*
REVISED 3/75

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION
Relocated Route 30 over I - 88
ALTERNATE TYPES OF ELASTOMERIC
EXPANSION JOINTS, ITEM 567.01
TYPE 200 ELASTOMERIC JOINT
No. 18 of 27

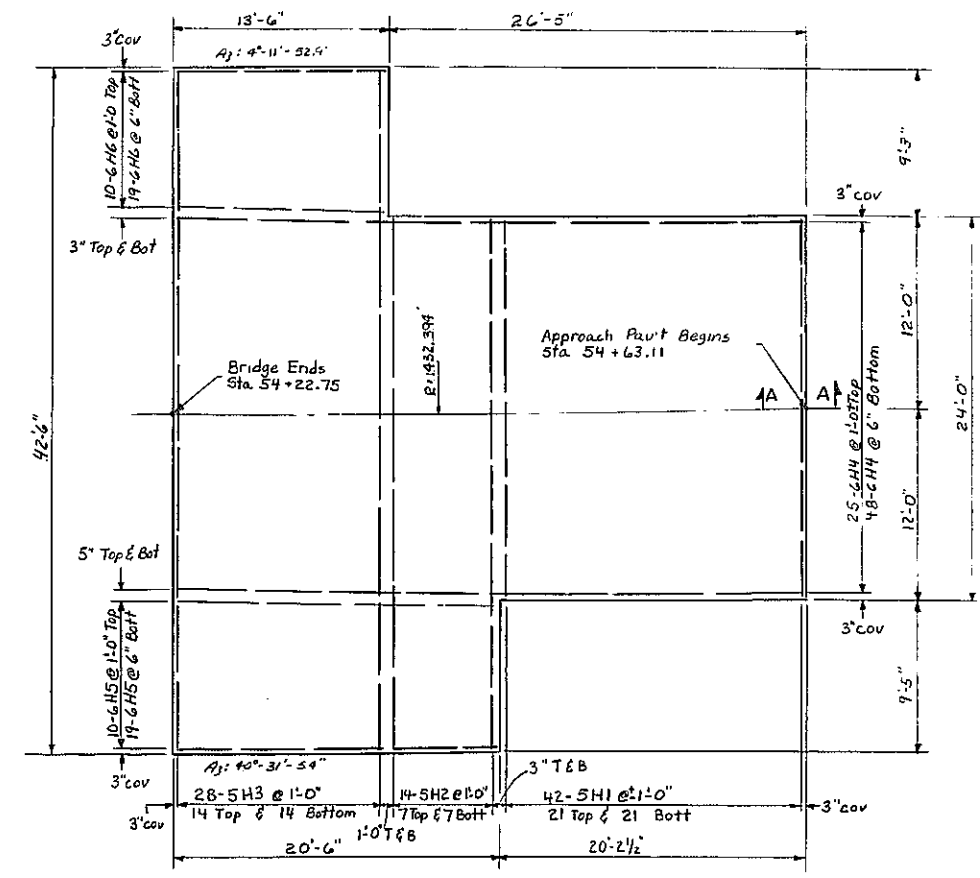


D95749

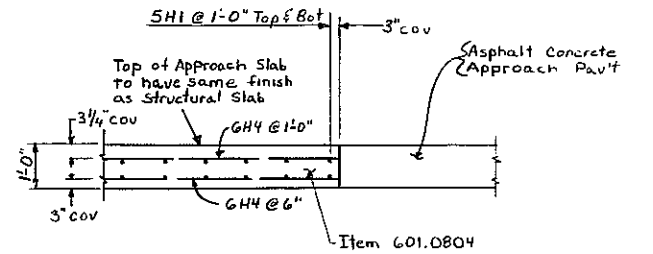
FED. RD. REG. NO.	STATE	FEDERAL AD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-1(52)	91	99
Interstate Route 508 Central Bridge to Schoharie County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				



SOUTH APPROACH SLAB
Scale 3/16" = 1'-0"

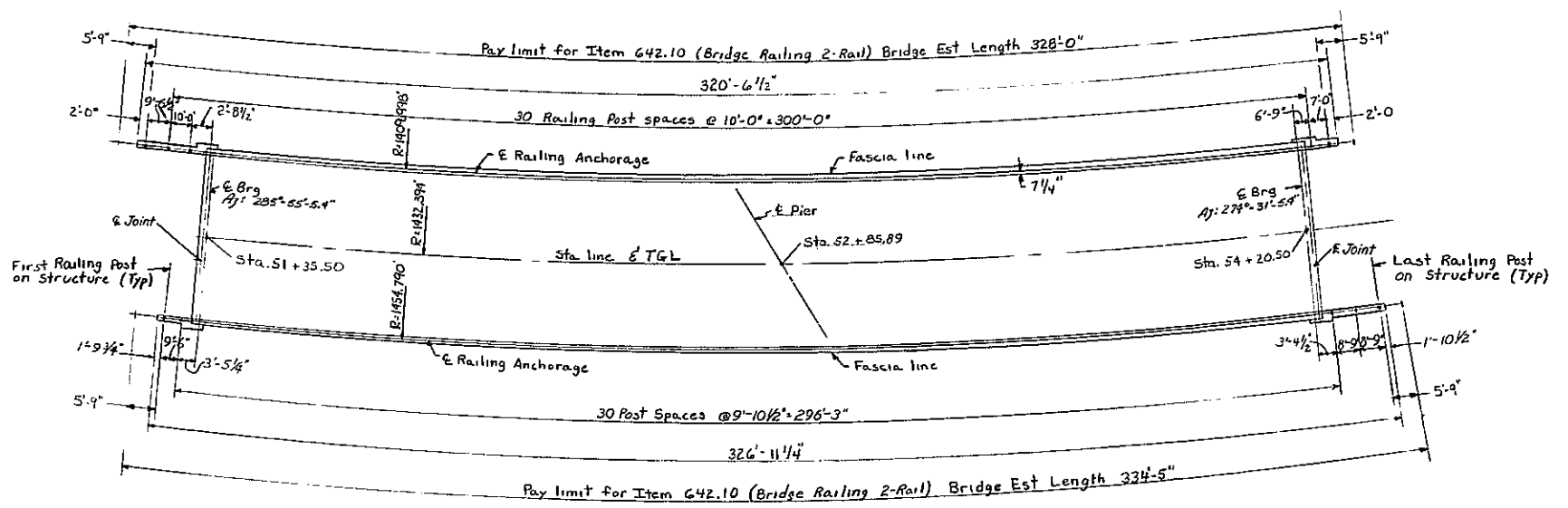


NORTH APPROACH SLAB
Scale 3/16" = 1'-0"



SECTION A-A
Scale 1/2" = 1'-0"

Pour	Item 601.0804	Item 602.0201
North Approach Slab	1285.43 S.F.	8539#
South Approach Slab	1287.56 S.F.	8521#
Totals	2572.99 S.F.	17065#



RAILING LAYOUT
Scale 1" = 20'-0"

DESIGNED BY: [REDACTED] CHECKED BY: [REDACTED] DETAILED BY: [REDACTED]

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I - 88

APPROACH SLAB AND RAILING LAYOUT

PROJ. ENG. [REDACTED]	DATE MADE 9/77
SQUAD [REDACTED]	DRAWING NO. 19 OF 27

Sod - from PC to PT = 10.57
Add in slope gutter



Sod - from PC to PT = 12.57
Add in slope gutter

D95749

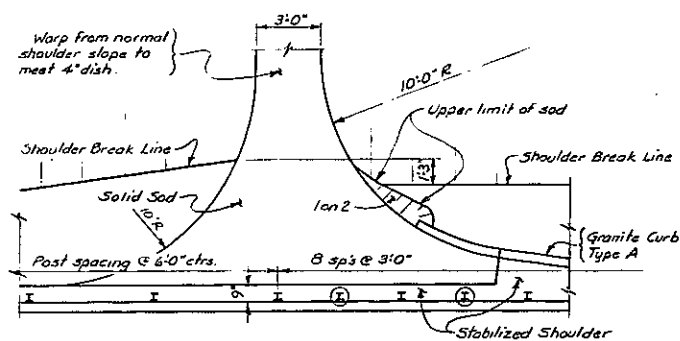
FED. RD. REG. NO.	STATE	FEDERAL AD PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	1-88-1(52)	92	99

Interstate Route 508
Central Bridge to Schenectady County Line
Schenectady County

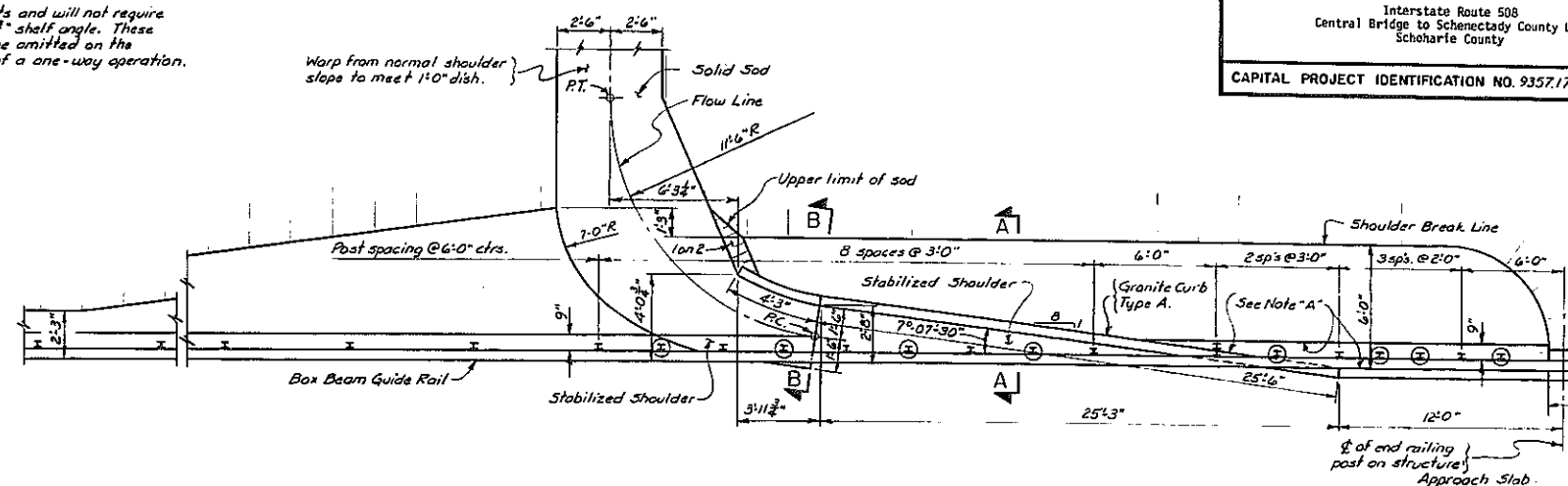
CAPITAL PROJECT IDENTIFICATION NO. 9357.17

Note: Except for sod gutter the details of the curb, guide rail and shoulder break lines are identical to those shown for the low end.

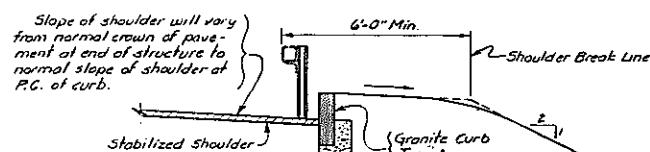
① These are backup posts and will not require use of the 5" x 3 1/2" x 3/8" shelf angle. These backup posts shall be omitted on the trailing or off end of a one-way operation.



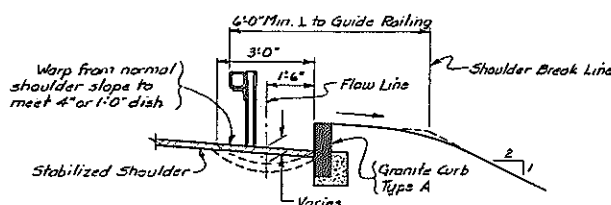
TYPE I SHOULDER - HIGH END
Scale 1/4" = 1'-0"



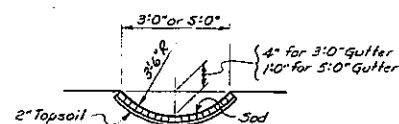
TYPE I SHOULDER - LOW END
Scale 1/4" = 1'-0"



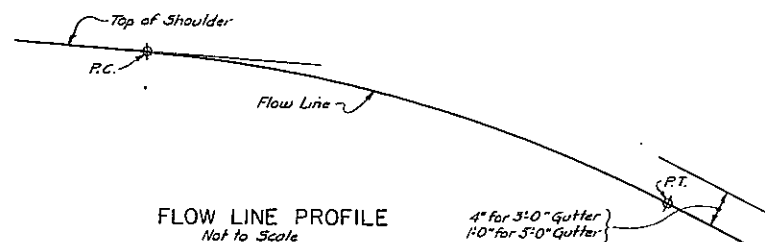
SECTION A-A
Not to Scale



SECTION B-B
Not to Scale



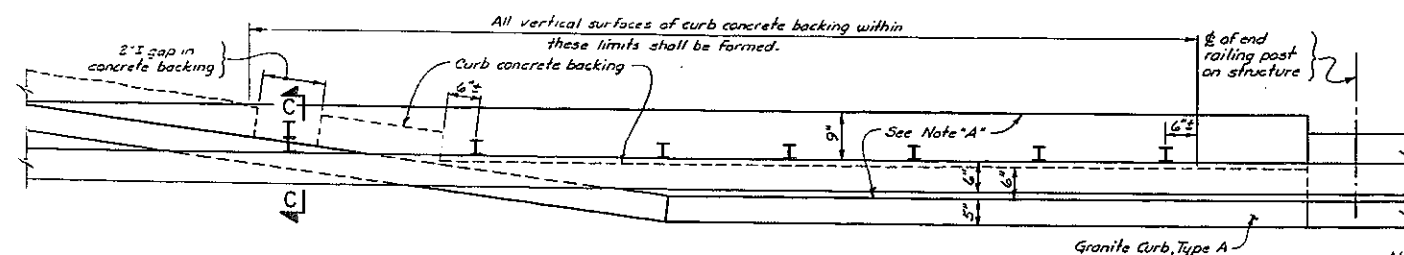
SECTION THRU GUTTER
Scale 1/2" = 1'-0"



FLOW LINE PROFILE
Not to Scale

Note "A"
Pave this area with the same material as in the stabilized shoulder. Payment will be made under the stabilized shoulder item.

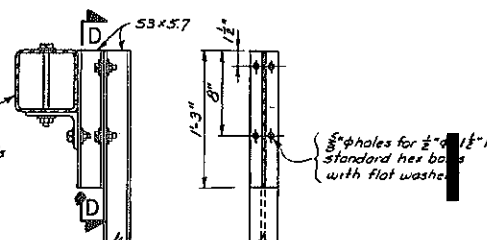
Notes:
For details of Granite Curb, Type A, see Highway Standard Sheet 609-1.



DETAIL OF CURB CONCRETE BACKING
Scale 1/2" = 1'-0"

Note:
The cost of furnishing and placing this special post with its stand-off shall be included in the unit price bid for the Box Beam Guide Rail item.

PARTIAL SECTION C-C
Scale 1/2" = 1'-0"



SECTION D-D
Scale 1/2" = 1'-0"

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I - 88

APPROACH DETAILS WITH BOX BEAM GUIDE RAILS

PROJ. ENR	DATE MADE 9/77
SQUAD	DRAWING NO. 20 OF 27

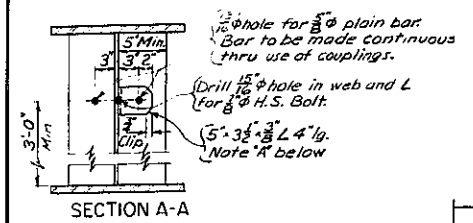
Check - Location of Catch Basins - if any

Type I Shoulder -



FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	I-88-115C	93	99
Interstate Route 508 Central Bridge to Schoharie County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				

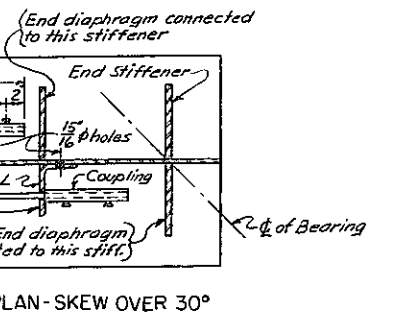
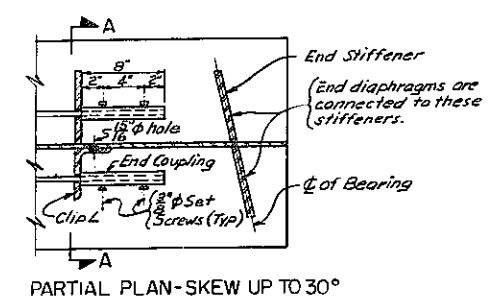
Handrail shall be placed thru stiffeners on both sides of Interior Girders and thru stiffeners on inside face of Fascia Girders.
The cost of furnishing and placing the bars, couplings, set screws, H.S. bolts and Clip Angles shall be paid for under item 616.01



Note A - Use clip angle midway between stiffeners if stiffener spacing is 8'-0" or greater.

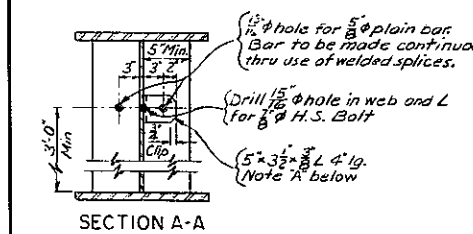
Provide a clip angle if a full depth stiffener is not required at this location.

When unpainted A588 steel is used in the superstructure, the hand rail shall be A588 and the couplings galvanized in accordance with specifications.



DETAILS FOR FIELD ERECTED SAFETY HANDRAIL
Not to Scale

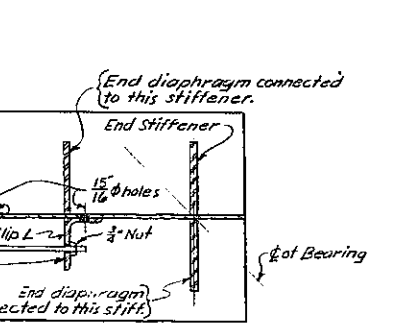
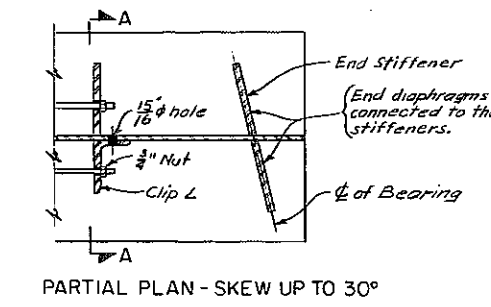
Handrail shall be placed thru stiffeners on both sides of Interior Girders and thru stiffeners on inside face of Fascia Girders.
The cost of furnishing and placing the bars, bar splices, nuts, H.S. Bolts and Clip Angles shall be paid for under item 616.01



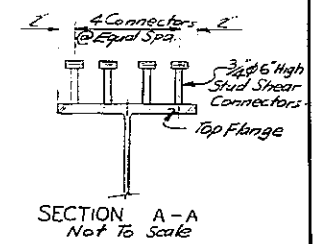
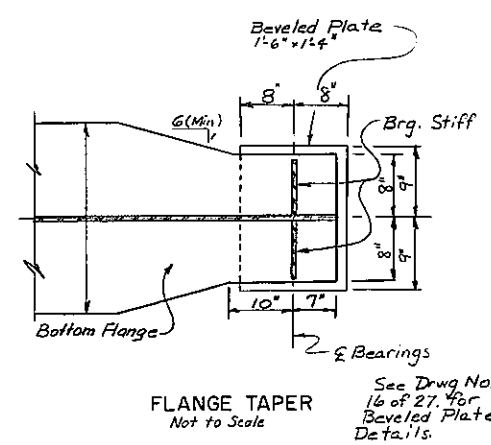
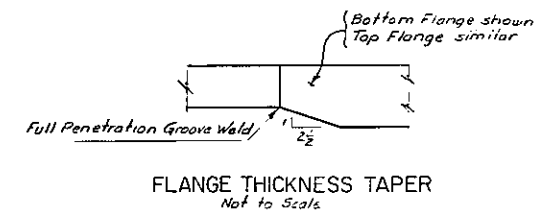
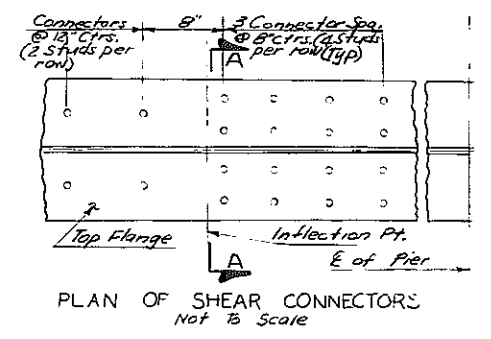
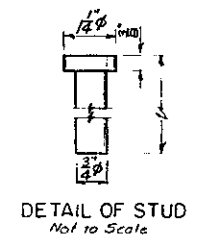
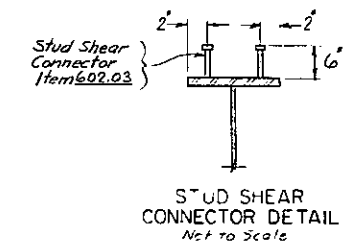
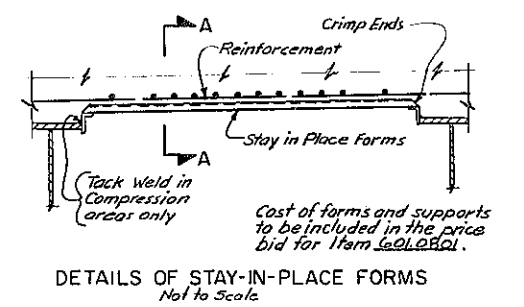
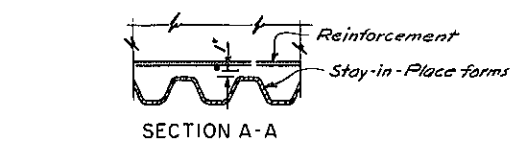
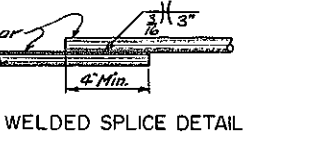
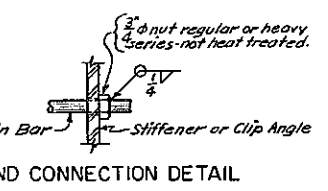
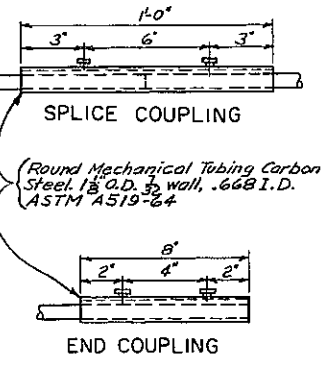
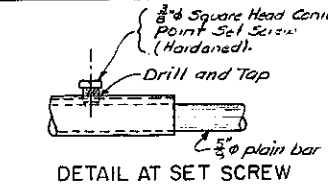
Note A - Use clip angle midway between stiffeners if stiffener spacing is 8'-0" or greater.

Provide a clip angle if a full depth stiffener is not required at this location.

When unpainted A588 steel is used in the superstructure, the hand rail shall be A588 and the couplings galvanized in accordance with specifications.

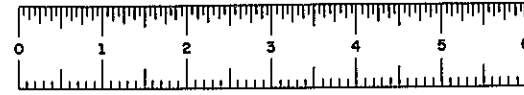


DETAILS FOR SHOP ERECTED SAFETY HANDRAIL
Not to Scale



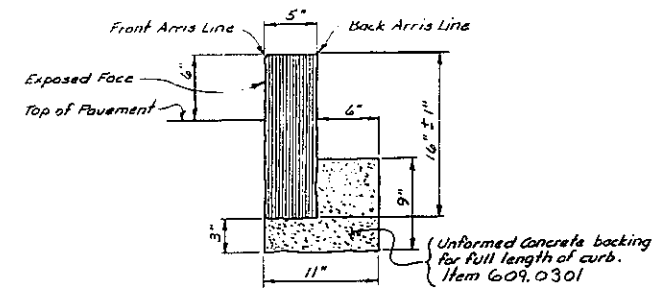
DATE MADE	1/77
PROJECT ENGINEER	
IN CHARGE OF	
DESIGNED BY	
DESIGN CHECKED BY	
DETAILED BY	
DETAIL CHECKED	

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION
Relocated Route 33
over
1 - 88
MISC. DETAILS
DRAWING NO 21 of 27

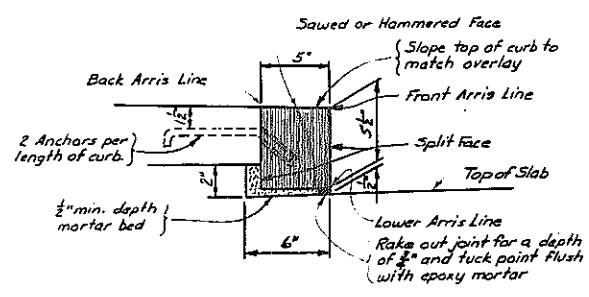


D95749

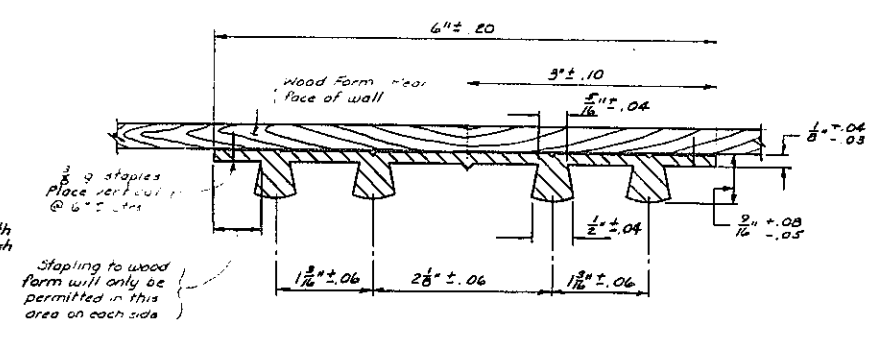
FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	I-88-1(62)	94	99
Interstate Route 508 Central Bridge to Schoharie County Line Schoharie County				
CAPITAL PROJECT IDENTIFICATION NO. 935717				



ITEM 609.0301
SAWED OR HAMMERED TOP QUARRY SPLIT FACE
DETAIL OF GRANITE CURB - TYPE A
(NON-MOUNTABLE)
Not to Scale

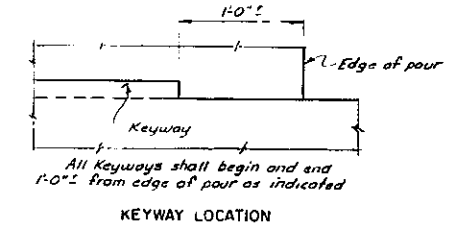
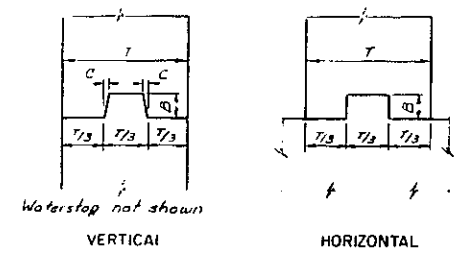


ITEM 609.0302
DETAIL OF GRANITE CURB - TYPE F1
Not to Scale
On Superstructure



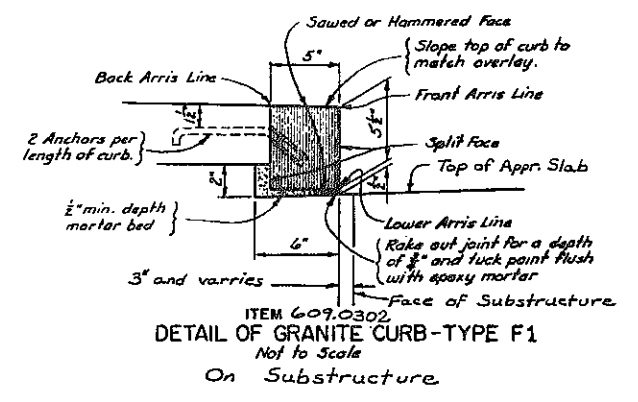
TYPE "D" WATERSTOP
Waterstops shall conform to the requirements of Section 605-11 of the Specifications.
Holes must not be made in waterstop for any purpose except as required for stapling to forms.
Type "D" waterstop shall be light gray in color.
The cost of furnishing and placing waterstops shall be included in the unit price bid for the concrete items.

NOTE
To facilitate shipping and handling of PVC waterstops, field butt splices will be permitted on straight runs at joints approved by the Engineer. Shop splices shall be used at locations shown on the Contract Plans. The method and equipment used to make the field splices shall be approved by the Deputy Chief Engineer (Structures).

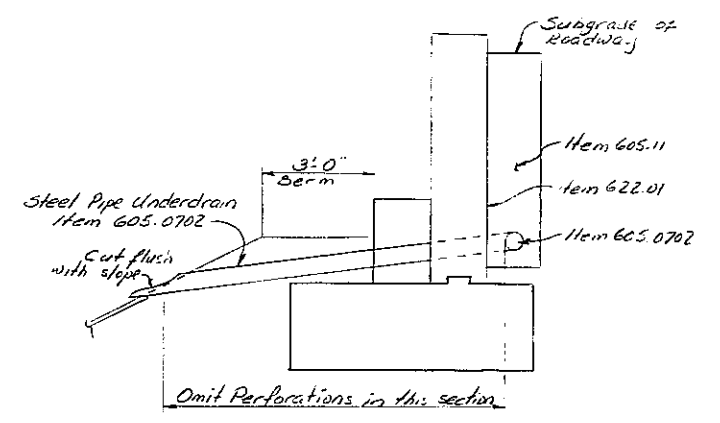


CONSTRUCTION & CONTRACTION JOINTS		
C	B	T/3
1/4"	1 1/2"	to 6"
1/2"	3 1/2"	6" to 10"
3/4"	5 1/2"	over 10"

EXPANSION JOINTS		
C	B	T/3
1/2"	3 1/2"	to 10"
3/4"	5 1/2"	over 10"

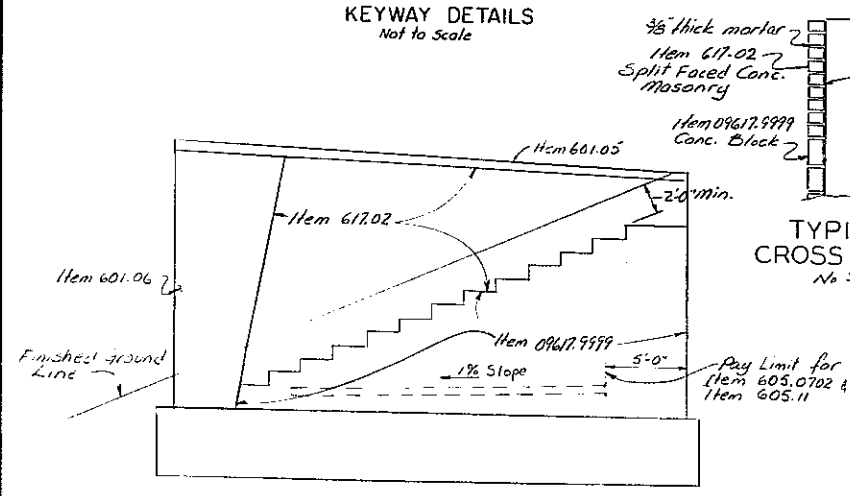


ITEM 609.0302
DETAIL OF GRANITE CURB - TYPE F1
Not to Scale
On Substructure

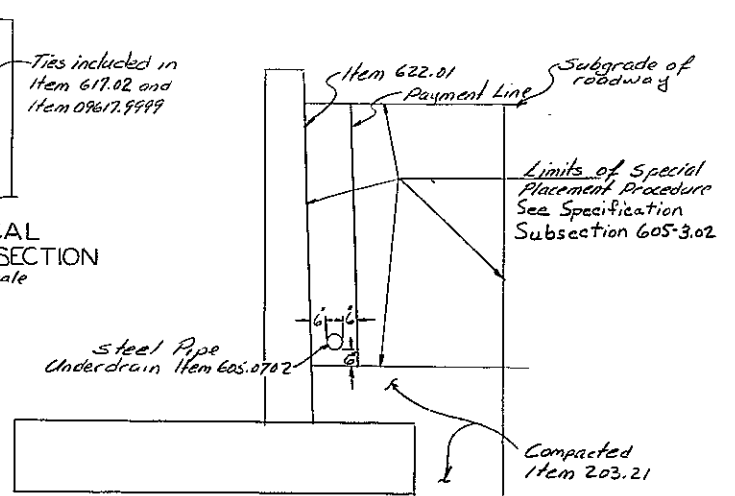


TYPICAL SECTION
No Scale

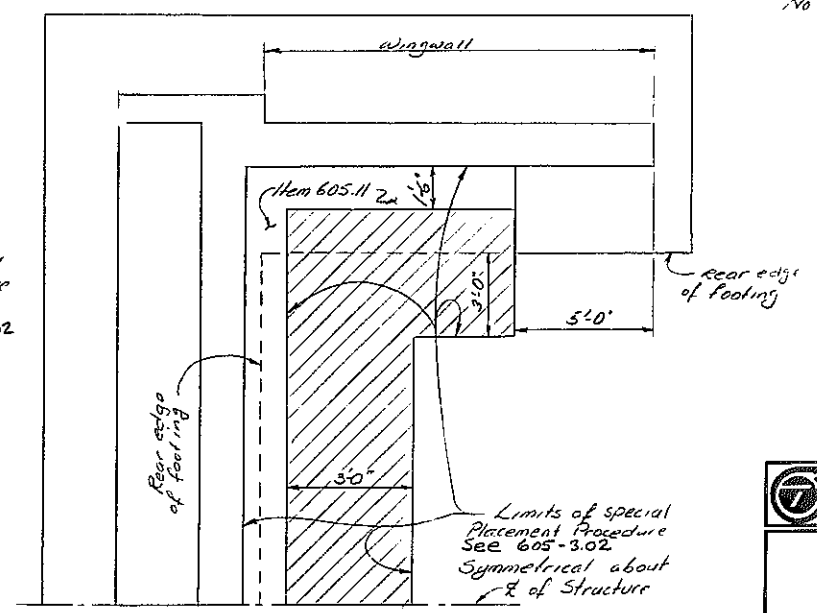
KEYWAY DETAILS
Not to Scale



SPLIT FACED MASONRY DETAIL
No Scale
Note: For Split Masonry Notes See Dwg. # 1 of 27



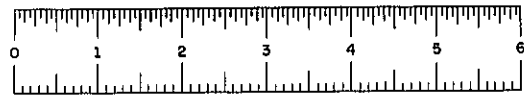
TYPICAL CROSS SECTION
No Scale



TYPICAL PLAN
For Placement Limits of Items 605.11 & 203.21 adjacent to structure
No Scale

DESIGNED BY: DETAILED BY: 1-77 DETAIL CHECKED BY: 1-77

STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION DIVISION OF CONSTRUCTION	
Relocated Route 30 over I - 88 MISC. DETAILS	
PROJ. ENG. SQUAD R	DATE MADE 9/77 DRAWING NO. 22 OF 27



D95749

FED RD REG NO	STATE	FEDERAL AID PROJECT NO	SHEET NO	TOTAL SHEETS
	NEW YORK	1-88-1(52)	96	99

Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County

CAPITOL PROJECT IDENTIFICATION NO. 935717

NOTES:
All railing is to be fabricated and erected so that the rails are parallel to each other and the posts are truly vertical.

The Base Plates shall be perpendicular to the post unless otherwise noted. When the railing is to be placed on a prepared surface, the Base Plate may be parallel to the grade or may be perpendicular to the post and made level by the use of (1:1 ratio cement to sand, mortar).

Tubular steel rails, rail clamp assemblies, nuts and washers, posts, post webs, if required, post spacers, railing joint assemblies and any necessary shims and mortar leveling course shall be paid for under the railing item.

Anchor studs, nuts and anchor plates shall be paid for under the railing item.

After the anchor stud nuts have been placed and tightened to the satisfaction of the Engineer, the studs shall be flame cut-off one inch above the nut and the first thread above the nut shall be damaged, as ordered by the Engineer.

Rails shall span a minimum of 3 posts. If this is impossible, the absolute minimum shall be 2 posts with one of these posts being a special post.

Materials used in the manufacture of this railing shall conform to the requirements and/or specifications listed below:

- Rail Tubes - ASTM Designation A500, Grade B
- Rail Clamps - ASTM Designation A500, Grade B, or A36 or A588.
- Clamp Bolts and Nuts - ASTM Designation A325
- Rail Splices - ASTM Designation A36 or A588
- Tubular Rail Splices - ASTM Designation A500, Grade B
- Channel Rail Splices - ASTM Designation A36
- Splice Plates - ASTM Designation A36
- 3/4" Post Plates - ASTM Designation A588
- 1" Post Plates - ASTM Designation A36
- Post Connection Plate - ASTM Designation A36 or A.I.S.I. Designation 1020
- Base Plates - ASTM Designation A588
- Splice Bolts, Nuts and Washers - ASTM Designation A307
- Anchor Studs - ASTM Designation A449
- Nuts and Washers for Anchor Studs - ASTM Designation A325
- Anchor Plates - ASTM Designation A36
- Post Web Plates (if required) - Same as Post Plate Material
- Plate Shims - ASTM Designation A36
- End Cap - ASTM Designation A36
- Galvanized Railing - All components of the railing, including anchor studs, nuts and washers shall be galvanized in accordance with Material Specification 719-01. Anchor Studs shall have a Class 2A thread fit prior to galvanizing. The cut portion of the anchor studs shall be repaired according to Material Specification 719-01.

All bolts shall have a Class 2A thread fit prior to galvanizing.

Grind all edges of Post Plates and Base Plates prior to galvanizing so that all sharp edges are removed.

Railing posts shall be erected to proper line and grade before concrete under post and in back of granite curb is poured unless otherwise indicated on the Contract Plans.

Unless covered by other Specifications, all dimensions related to the fabrication of the steel railing shall have a tolerance of ± 1/16".

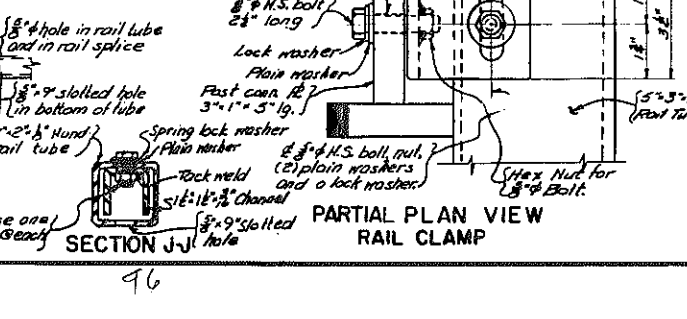
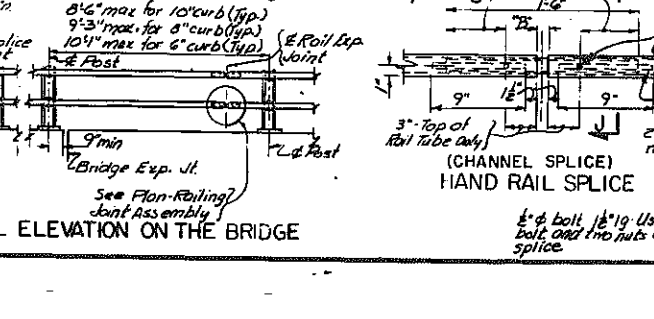
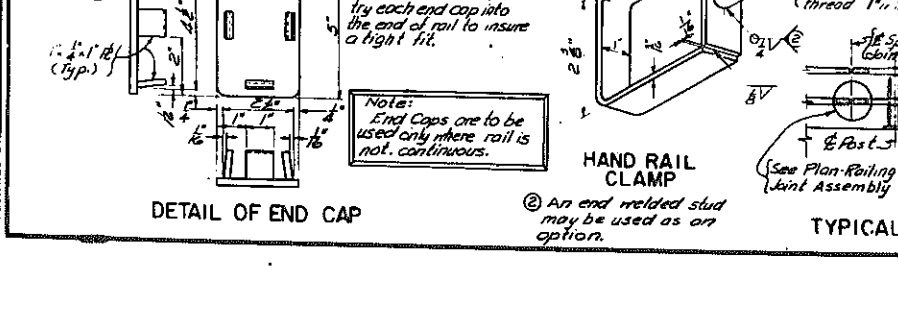
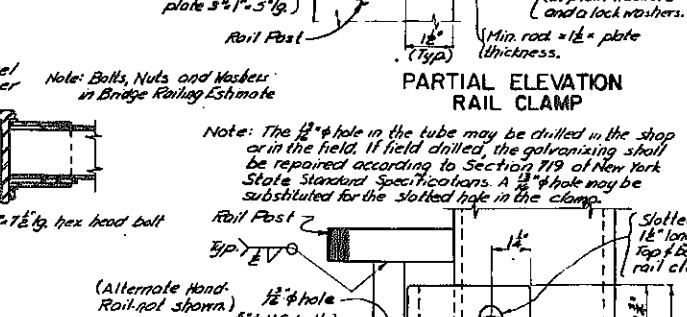
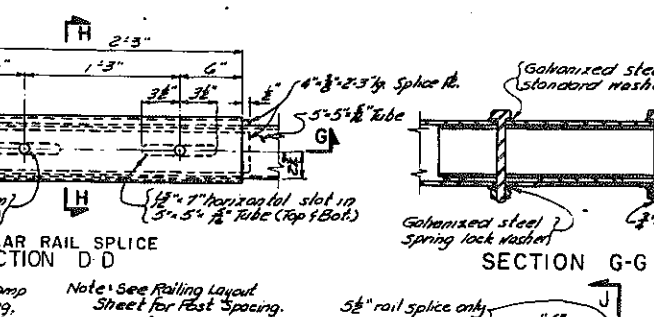
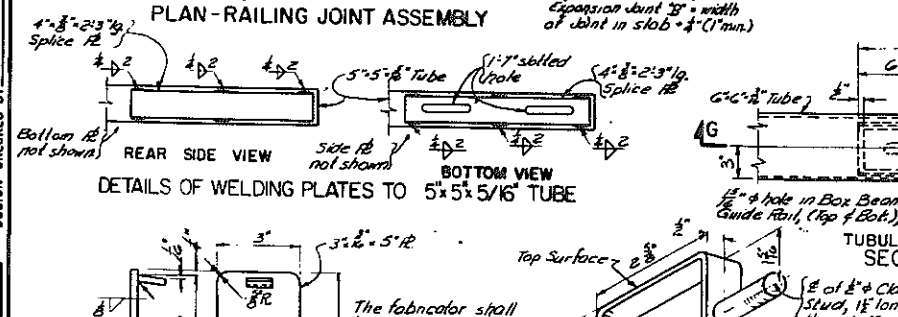
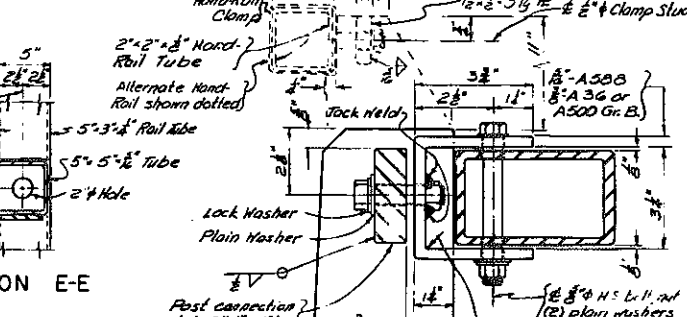
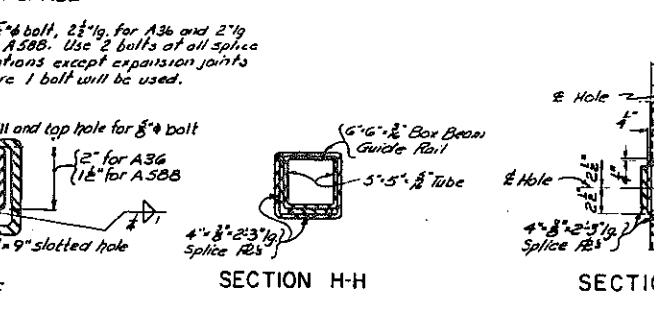
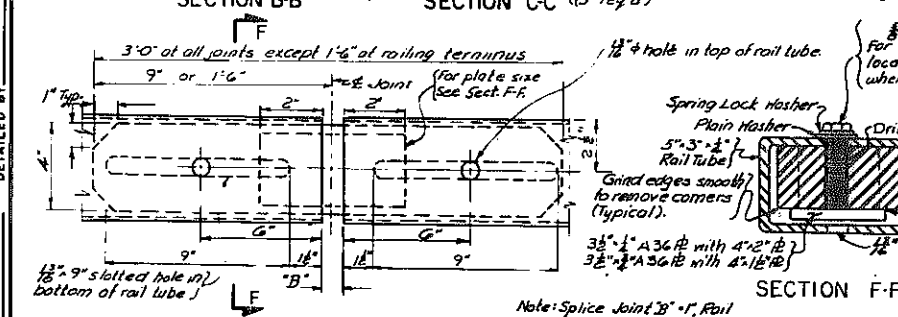
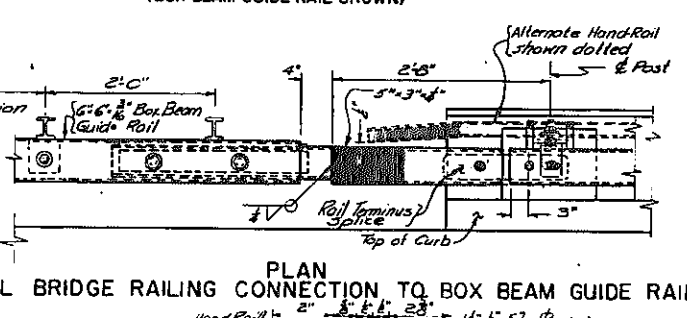
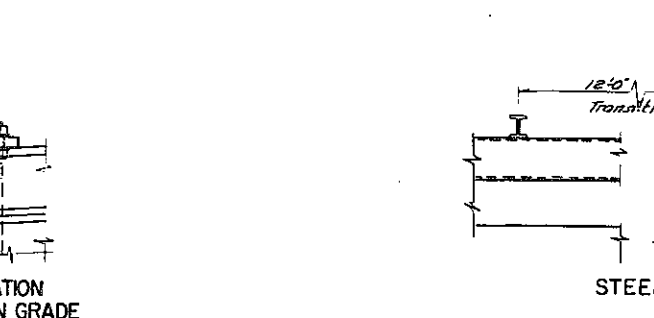
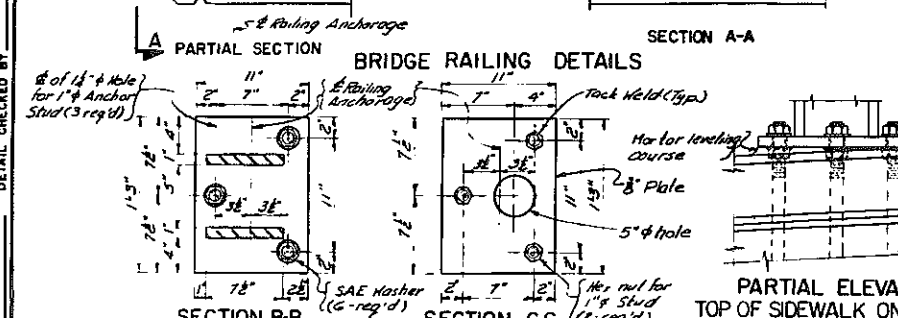
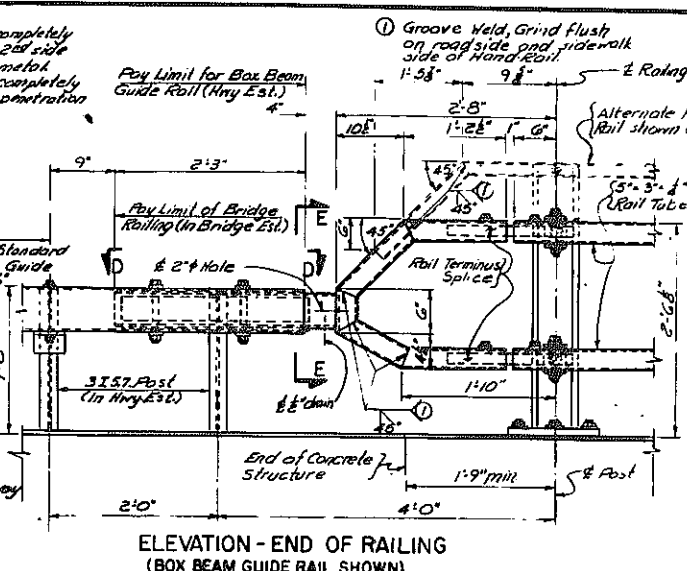
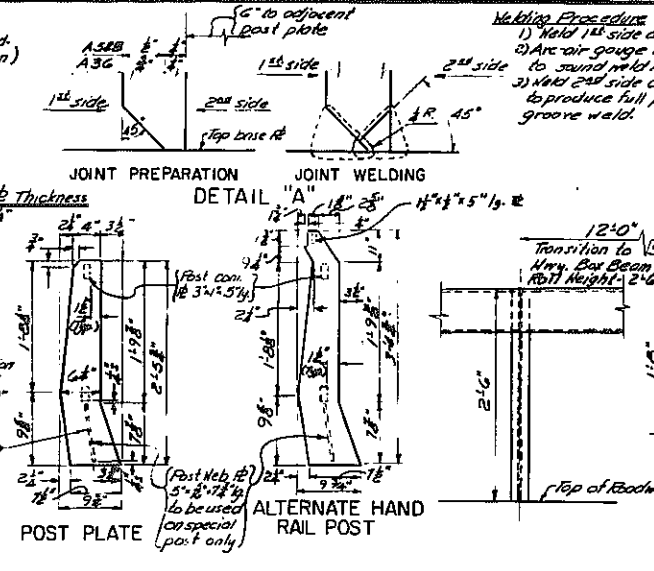
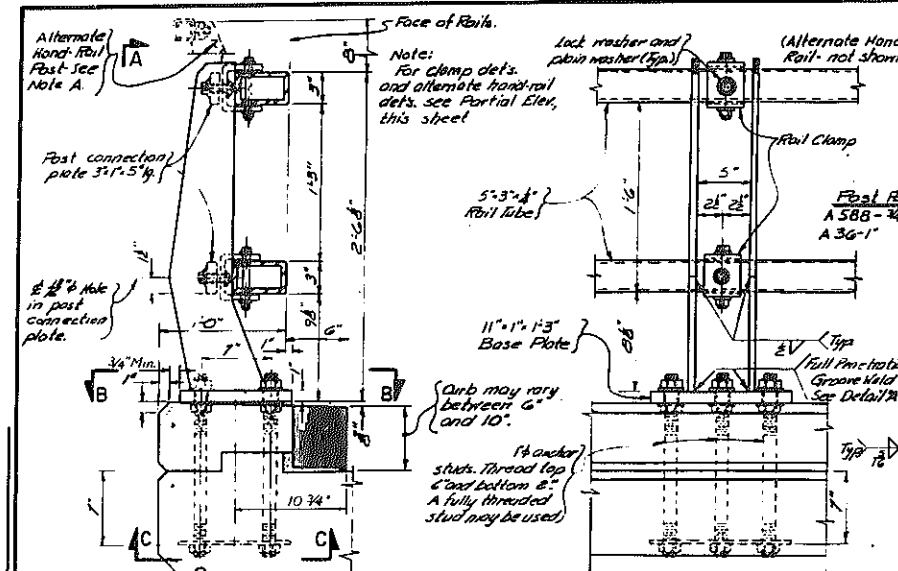
If the end of the bolts connecting the clamp to the post connection plate bear against the tube when in the final position, additional plain washers shall be added to prevent the end of the bolt from bearing against the tube.

Other clamp steels may be used with the approval of the Deputy Chief Engineer (Structures).

All high-strength bolts shall be torqued snug tight (approximately 100 ft. lbs.)

The hole for the bolt connecting the tube end and the clamp will be located in the tube so that the tube will bear against the post plate when in the final position.

NOTE "A":
The alternate handrail is for use when the traffic railing system is used between pedestrian traffic and vehicular traffic.

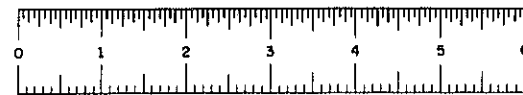


STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I-88

STEEL BRIDGE RAILING-TWO RAIL

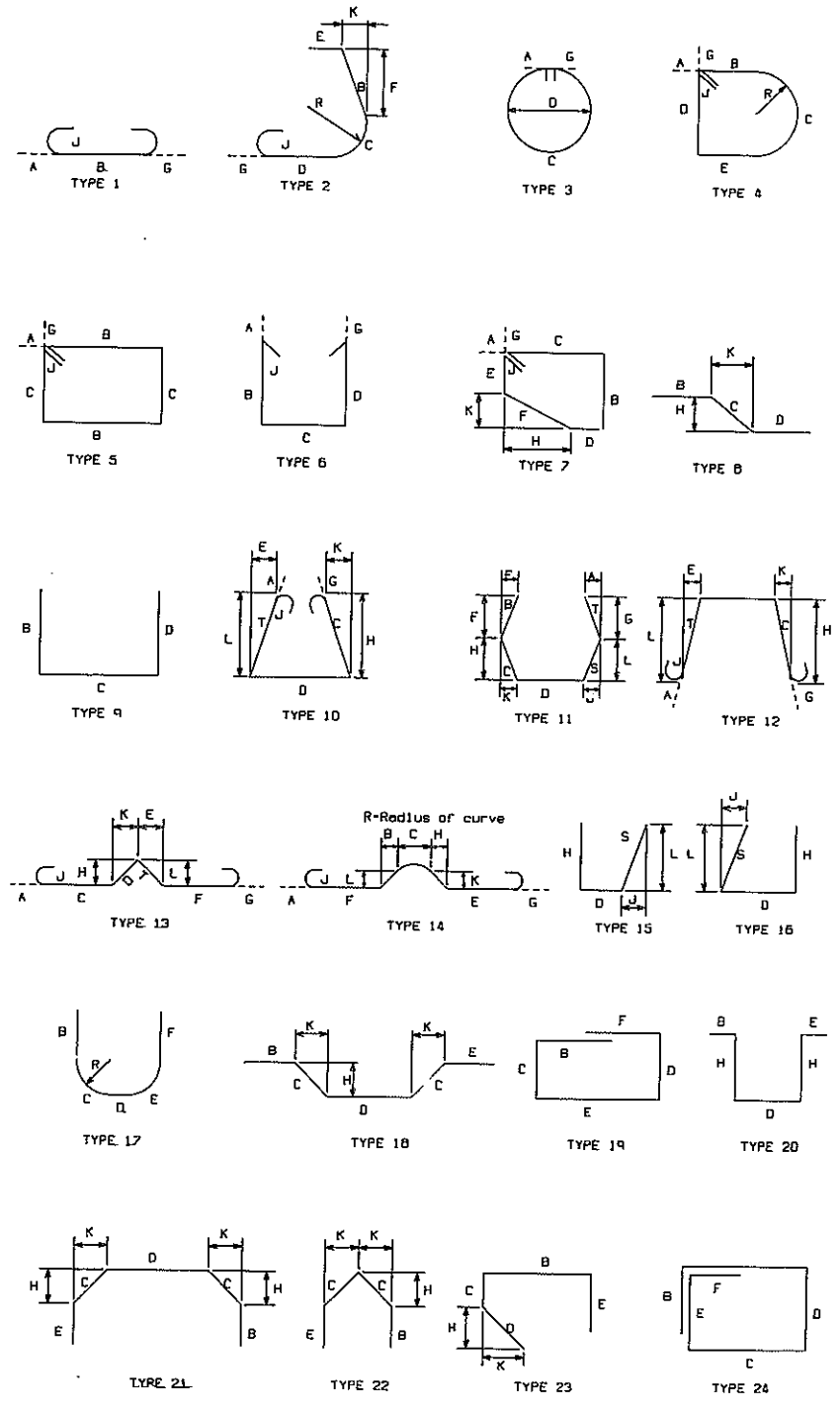
PROJ. ENG. DATE MADE 9/17
SQUAD DRAWING NO. 24 OF 27



D95749

FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	I-88-1(62)	97	99
Interstate Route 508 Central Bridge to Schenectady County Line Schoharie County				
CAPITOL PROJECT IDENTIFICATION NO. 9357-17				

MARK	SZ.	NO.	LENGTH	TYP	A	B	C	D	E	F	G	H	J	K	L	R	S	T	WEIGHT
FOOTINGS POUR I NORTH ABUT.																			
SA1	5	12	48-5	STR															606
SA2	5	49	7-1	STR															382
SA3	5	10	25-10	STR															269
SA4	5	25	6-1	STR															159
SA5	5	10	18-10	STR															196
SA6	5	12	4-11	STR															62
SA7	5	40	4-1	1	0-7	3-6					***								170
BA8	6	11	4-7	1	0-8	3-11					***								76
BA9	6	10	6-3	STR															94
SUBTOTAL UNCOATED BARS - THIS POUR -																			1994
STEM - LOWER WINGWALLS POUR II																			
SA10	5	24	11-2	STR															260
LENGTH VARIES FROM 9-10 TO 12-6																			
SA11	5	5	12-5	STR															65
SA12	5	23	4-4	STR															104
LENGTH VARIES FROM 2-11 TO 5-9																			
SA14	5	11	48-0	STR															551
SA15	5	31	3-8	STR															119
BA16	6	5	47-11	STR															640
BA17	6	11	13-7	STR															224
LENGTH VARIES FROM 13-5 TO 13-9																			
SA18	5	10	13-7	STR															142
LENGTH VARIES FROM 13-5 TO 13-9																			
SA19	5	5	20-7	STR															107
SA20	5	7	23-8	STR															173
SA21	5	8	13-5	STR															84
SA22	5	7	4-10	STR															35
LENGTH VARIES FROM 4-4 TO 5-4																			
SA23	5	11	11-0	STR															126
LENGTH VARIES FROM 10-10 TO 11-1																			
SA24	5	8	11-0	STR															91
LENGTH VARIES FROM 10-10 TO 11-1																			
SA25	5	3	13-7	STR															43
SA26	5	4	16-8	STR															70
SA27	5	6	10-9	STR															67
SA28	5	4	5-0	STR															21
LENGTH VARIES FROM 4-9 TO 5-3																			
SA29	5	30	2-8	q			2-2	0-6	***										83
SUBTOTAL UNCOATED BARS - THIS POUR -																			3026
POUR II UPPER WINGWALLS - BACKWALL																			
SA30	5	2	7-2	STR															15
SA31	5	18	7-5	STR															15
SA31	5	18	7-2	STR															135
LENGTH VARIES FROM 6-11 TO 7-6																			
SA32	5	4	7-2	STR															30
LENGTH VARIES FROM 7-0 TO 7-5																			
SA33	5	15	48-0	STR															751
SA34	5	6	20-7	STR															129
SA35	5	8	23-8	STR															197
SA36	5	8	5-11	STR															50
LENGTH VARIES FROM 5-4 TO 6-7																			
SA37	5	6	13-7	STR															85
SA38	5	9	16-8	STR															156
SA39	5	8	5-11	STR															49
LENGTH VARIES FROM 5-3 TO 6-7																			
SA40	5	45	4-0	q			1-10	0-4	1-10										188
SA41	5	27	4-9	q			2-0	0-4	2-0										134
SA42	5	10	5-10	q			2-0	1-10	2-0										61
SA43	5	6	4-8	q			2-0	0-8	2-0										29
SUBTOTAL UNCOATED BARS - THIS POUR -																			2024
POUR IV PEDESTALS																			
SA44	5	10	10-0	5	0-6	2-4	2-2												108
SUBTOTAL UNCOATED BARS - THIS POUR -																			104



BAR BENDING DIAGRAMS
All bar dimensions out to out unless otherwise indicated

EXPLANATION OF BAR MARKS

First or first and second characters indicate size of bar.
First alphabetic character indicates structure unit.
If followed by the letter "E" - bar is Epoxy Coated.
If followed by the letter "G" - bar is Galvanized.
Remainder is sequential listing of bar marks.

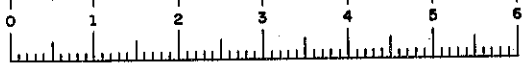
STRUCTURE UNIT

- A-Abutment
- C-Culvert
- F-Foundation Pile
- H-Highway Approach Slab
- P-Pier
- R-Rigid Frame Arch
- S-Superstructure
- W-Wall (Isolated)

DESIGNED BY _____ CHECKED BY _____

STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
DIVISION OF CONSTRUCTION

Relocated Route 30
over
I - 88
BAR LIST



D95749

FED. RD. PEG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK	L-88-1(5)	99	99

Interstate Route 508
Central Bridge to Schoharie County Line
Schoharie County

CAPITOL PROJECT IDENTIFICATION NO. 9357-17

MARK	SZ.	NO.	LENGTH	TYP	A	B	C	D	E	F	G	H	J	K	L	R	S	T	WEIGHT
WINGWALL OVERLAYS POUR B																			
5A45	5	1	22-8	STR															24
5A46	5	1	25-2	STR															26
5A53	5	1	12-2	STR															13
5A54	5	1	15-8	STR															18
5A55	5	2	6-8	STR															14
SUBTOTAL - UNCOATED BARS - THIS POUR -																			93
NORTH APPROACH SLAB																			
5H1	5	42	23-6	STR															1030
5H2	5	14	32-11	STR															481
5H3	5	28	42-0	STR															1222
6H4	6	73	39-9	STR															4363
6H4	VARIES FROM 40-2 TO 39-5 (1 SET OF 25) (1 SET OF 48)																		
6H5	6	29	20-0	STR															971
6H6	6	29	13-0	STR															566
SUBTOTAL - UNCOATED BARS - THIS POUR -																			8538
SOUTH APPROACH SLAB																			
5H1	5	35	23-6	STR															882
5H2	5	20	32-9	STR															683
5H3	5	26	42-0	STR															1139
6H4	6	73	39-9	STR															4363
6H4	VARIES FROM 39-5 TO 40-2 - 39-9 (1 SET OF 25) (1 SET OF 48)																		
6H5	6	29	12-0	STR															523
6H6	6	29	21-6	STR															937
SUBTOTAL - UNCOATED BARS - THIS POUR -																			8527
SUPERSTRUCTURE SLAB																			
EPOXY COATED																			
5SE1	5	165	58-6	STR															10061
5SE1	LENGTH VARIES FROM 57-8 TO 59-3 (5 SETS OF 33)																		
7SE2	7	30	60-0	STR															3680
7SE3	7	60	22-6	STR															2781
5SE4	5	496	46-10	1	0-7	45-8					0-7		0-5						24227
SUBTOTAL - EPOXY COATED BARS - THIS POUR -																			40749
UNCOATED																			
5S5	5	260	58-6	STR															15854
5S5	LENGTH VARIES FROM 57-3 TO 59-3 (5 SETS OF 52)																		
5S6	5	500	45-8	STR															23818
5S7	5	20	7-8	STR															180
5S8	5	80	3-0	15				1-2				0-9	0-9		0-9		1-1		248
5S9	5	808	4-2	20			1-3		1-2	1-3			0-3						3512
5S10	5	572	4-6	24			0-6	1-0	0-9	0-9	0-6								2685
SUBTOTAL - UNCOATED BARS - THIS POUR -																			46277
SAFETY WALK																			
5S11	5	10	52-8	STR															602
5S12	5	10	59-3	STR															618
SUBTOTAL - UNCOATED BARS - THIS POUR -																			1220
TOTAL UNCOATED BARS IN SUPERSTRUCTURE -																			47497
TOTAL EPOXY COATED BARS IN SUPERSTRUCTURE -																			40749
TOTAL UNCOATED BARS IN SUBSTRUCTURE -																			36901
TOTAL UNCOATED BARS IN MISCELLANEOUS -																			17055
TOTAL UNCOATED BARS IN ENTIRE STRUCTURE -																			101483
TOTAL EPOXY COATED BARS IN ENTIRE STRUCTURE -																			40749

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over
1 - 88

Bar List

PROJ. SQUAD _____ DATE MADE 10/14/77
DRAWING NO. 27 OF 27